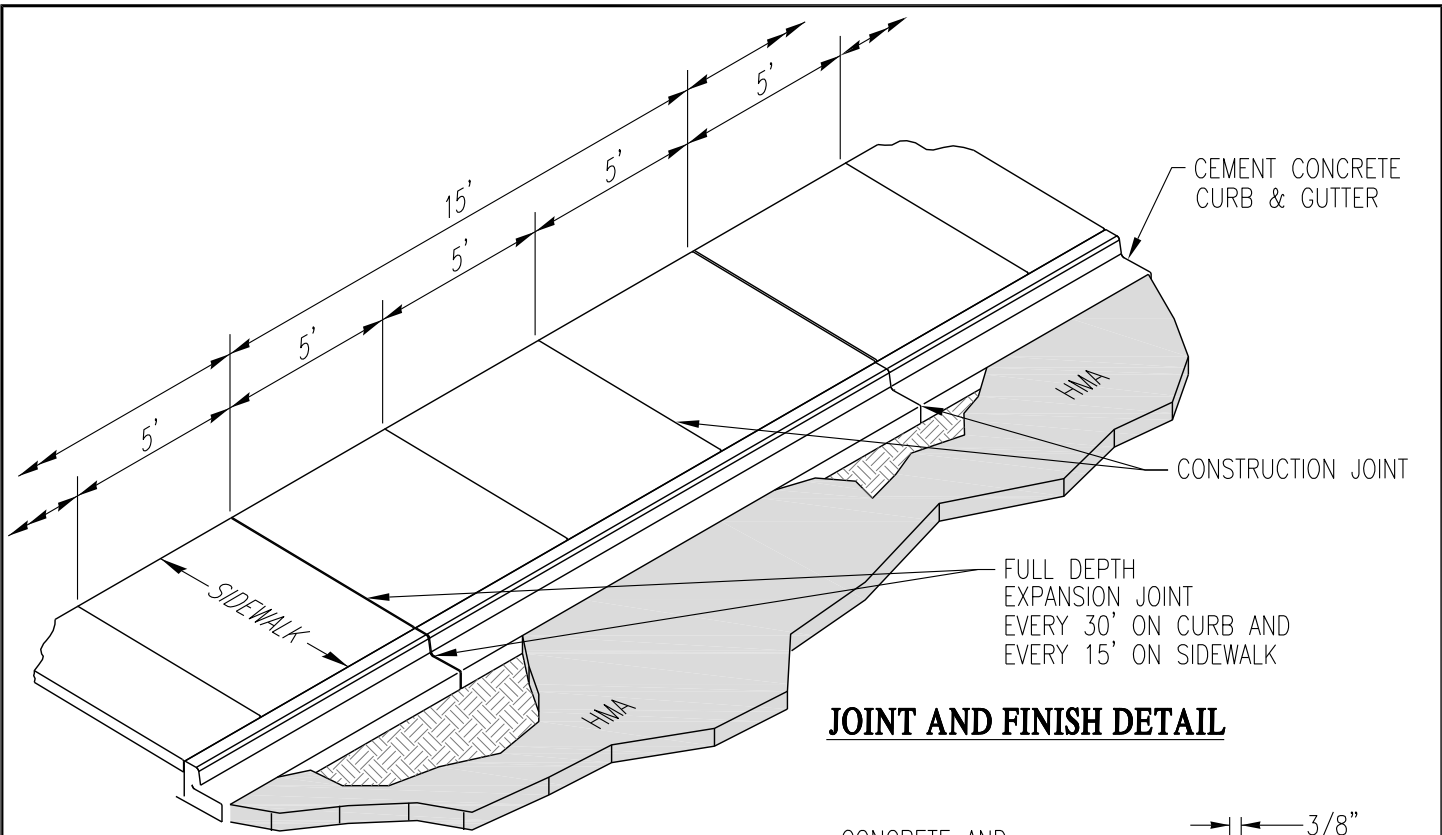
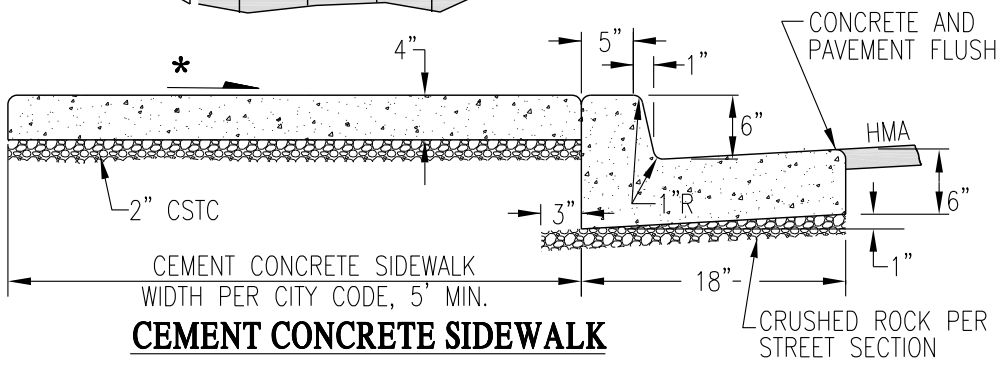


# Standard Details

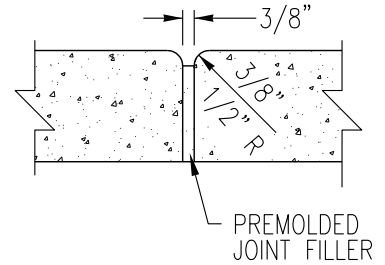
## Streets



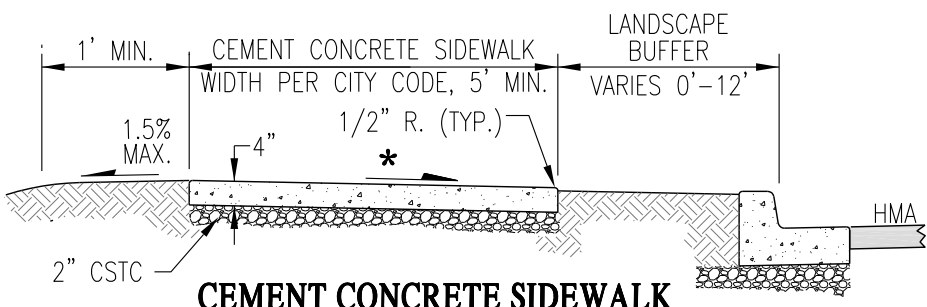
**JOINT AND FINISH DETAIL**



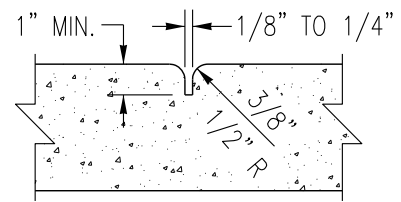
**CEMENT CONCRETE SIDEWALK**



**EXPANSION JOINT**



**CEMENT CONCRETE SIDEWALK**



**CONSTRUCTION JOINT**

**LEGEND:**  
 ← SLOPE IN EITHER DIRECTION  
 \* 1.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (2% MAX)

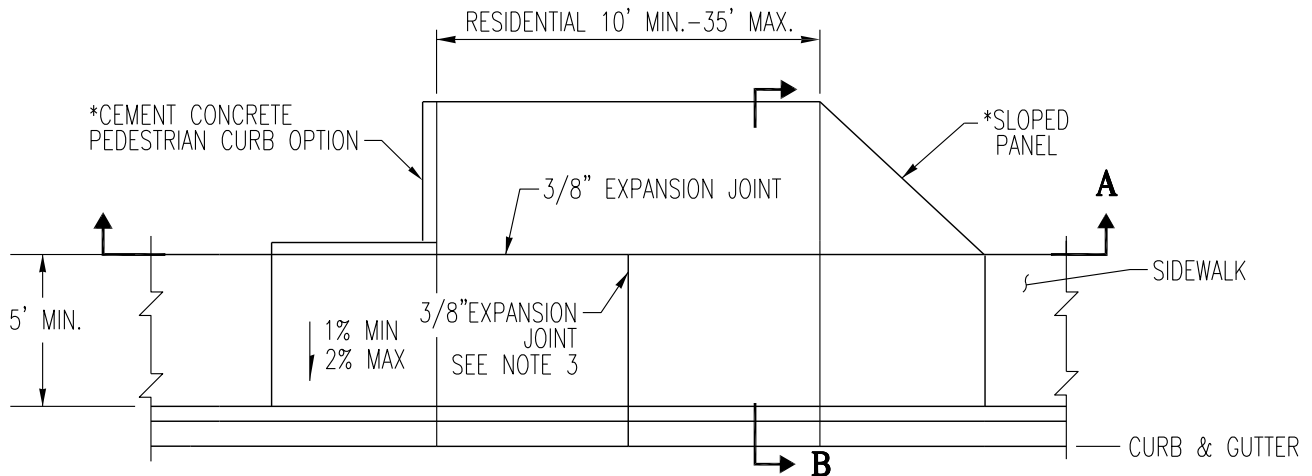
**NOTES:**

1. SEE STANDARD DETAIL ST7 FOR CURB, GUTTER AND SIDEWALK NOTES.
2. INSTALL 3/8" FULL EXPANSION JOINTS IN CURB AT ALL PC'S, PT'S AND CURB RETURNS.
3. ALL FILETS 1/2" UNLESS OTHERWISE NOTED.
4. FINISHED GRADE ADJACENT TO SIDEWALK OR CURB SHALL BE 1" BELOW TOP OF CONCRETE SURFACE.

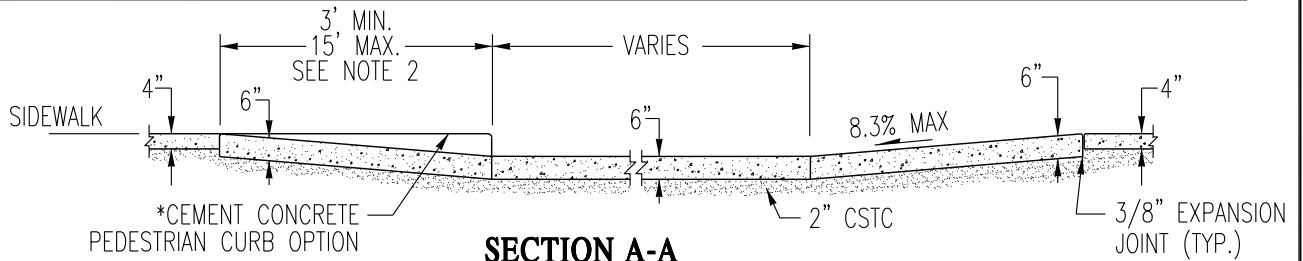


**CURB, GUTTER & SIDEWALK**

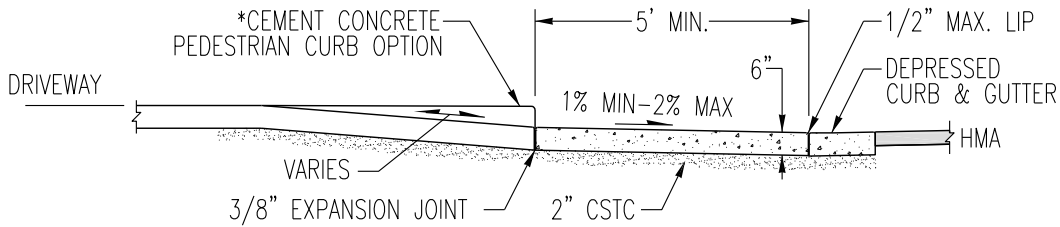
PUBLIC WORKS ENGINEERING	
APPR. BY: PKR	DATE: 05.18
DRAWN BY: EY	DWG: ST1
CAD FILE: 2013_ST1_05_2018	



**CEMENT CONCRETE DRIVEWAY**



**SECTION A-A**



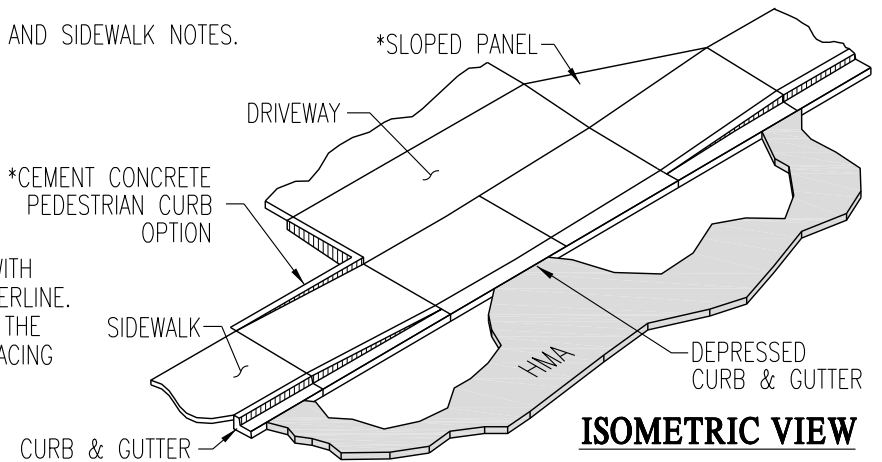
**SECTION B-B**

**\*NOTE:**  
 OPTION USED WILL  
 BE DETERMINED BY  
 CITY ENGINEER;  
 DEPENDENT UPON  
 FIELD CONDITIONS.

**LEGEND:** SLOPE IN EITHER DIRECTION

**NOTES:**

1. SEE STANDARD DETAIL ST7 FOR CURB, GUTTER AND SIDEWALK NOTES.
2. THE CURB RAMP MAXIMUM RUNNING SLOPE SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15' TO AVOID CHASING THE SLOPE INDEFINITELY WHEN CONNECTING TO STEEP GRADES.
3. WHEN THE DRIVEWAY WIDTH EXCEEDS 15', CONSTRUCT A FULL DEPTH EXPANSION JOINT WITH 3/8" JOINT FILLER ALONG THE DRIVEWAY CENTERLINE. CONSTRUCT EXPANSION JOINTS PARALLEL WITH THE CENTERLINE AS REQUIRED AT 15' MAXIMUM SPACING WHEN DRIVEWAY WIDTHS EXCEED 30'.
4. SEE RMC 12.04.090 AND 12.04.120 FOR ADDITIONAL DETAILS

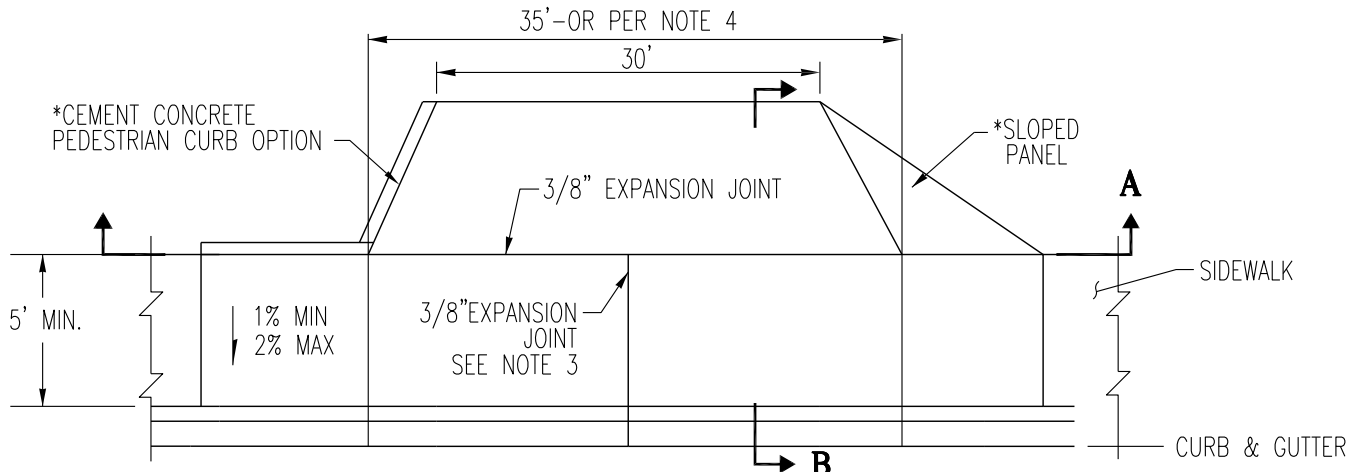


**ISOMETRIC VIEW**

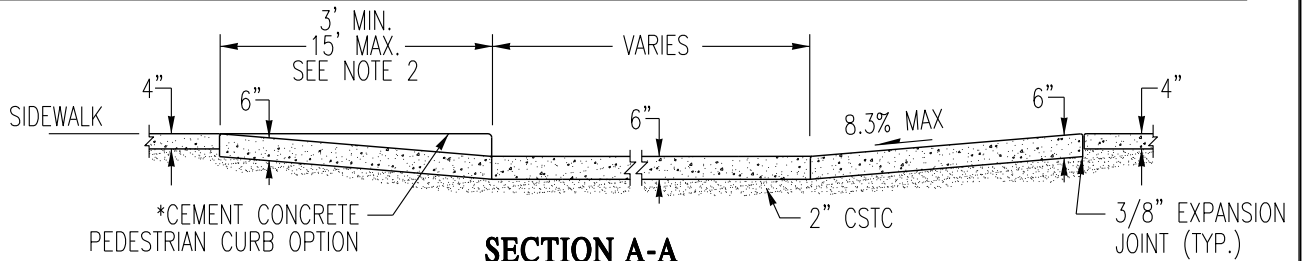


**STANDARD  
 RESIDENTIAL  
 DRIVEWAY (TYPE 1)**

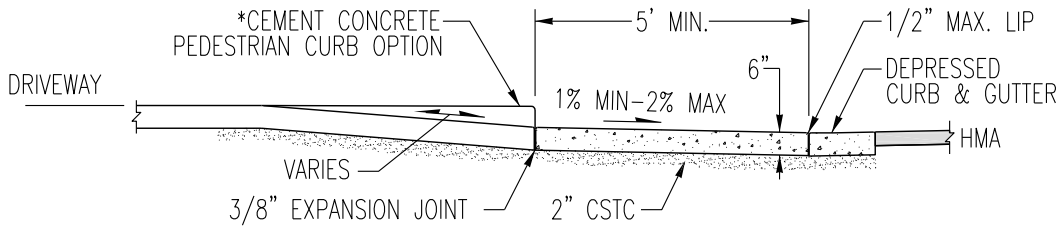
PUBLIC WORKS ENGINEERING	
APPR. BY: PKR	DATE: 06.19
DRAWN BY: EY	DWG: ST2
CAD FILE: 2013_ST2_06_2019	



**CEMENT CONCRETE DRIVEWAY**



**SECTION A-A**



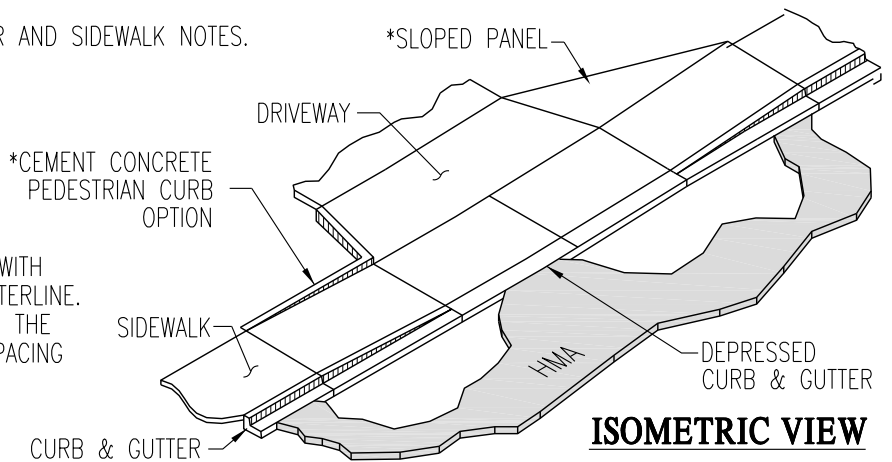
**SECTION B-B**

**LEGEND:** SLOPE IN EITHER DIRECTION

**\*NOTE:**  
OPTION USED WILL  
BE DETERMINED BY  
CITY ENGINEER;  
DEPENDENT UPON  
FIELD CONDITIONS.

**NOTES:**

1. SEE STANDARD DETAIL ST7 FOR CURB, GUTTER AND SIDEWALK NOTES.
2. THE CURB RAMP MAXIMUM RUNNING SLOPE SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15' TO AVOID CHASING THE SLOPE INDEFINITELY WHEN CONNECTING TO STEEP GRADES.
3. WHEN THE DRIVEWAY WIDTH EXCEEDS 15', CONSTRUCT A FULL DEPTH EXPANSION JOINT WITH 3/8" JOINT FILLER ALONG THE DRIVEWAY CENTERLINE. CONSTRUCT EXPANSION JOINTS PARALLEL WITH THE CENTERLINE AS REQUIRED AT 15' MAXIMUM SPACING WHEN DRIVEWAY WIDTHS EXCEED 30'.
4. SEE RMC 12.04.095 THRU 12.04.120 FOR ADDITIONAL DETAILS



**ISOMETRIC VIEW**



**STANDARD  
NON-RESIDENTIAL  
DRIVEWAY (TYPE 1)**

PUBLIC WORKS ENGINEERING

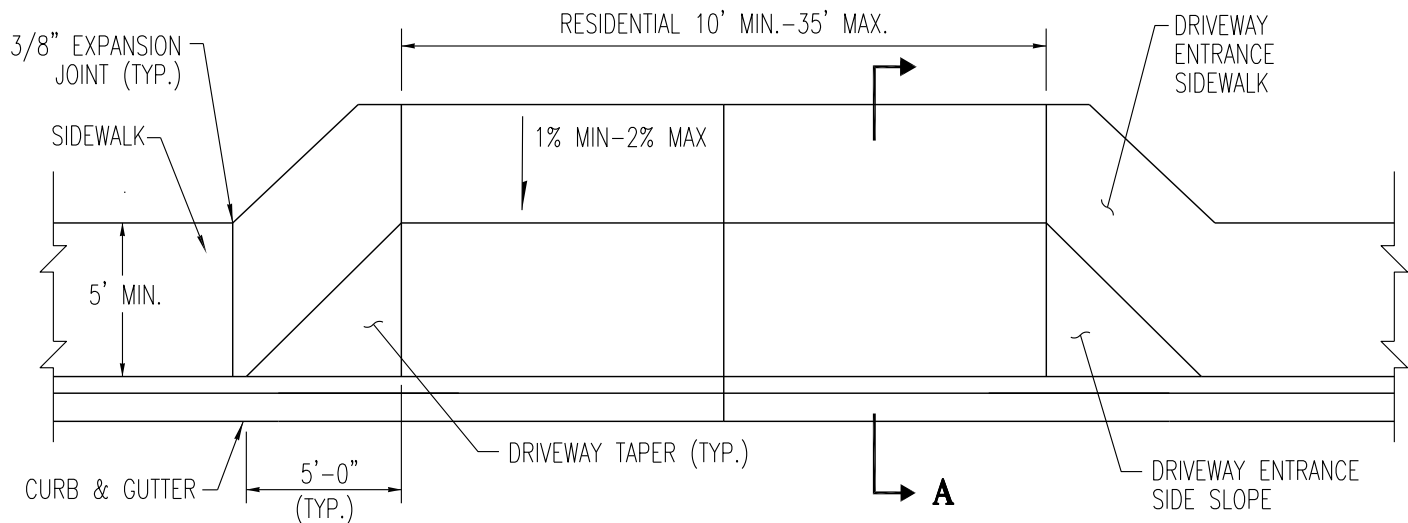
APPR. BY: PKR

DATE: 06.19

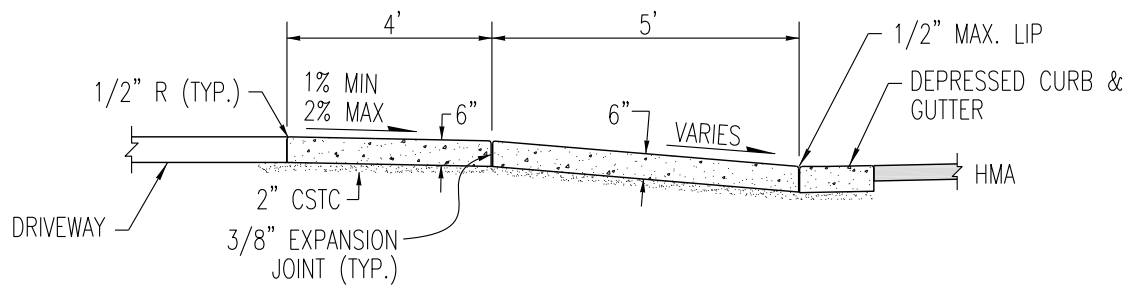
DRAWN BY: EY

DWG: ST2A

CAD FILE: 2013\_ST2A\_06\_2019



**CEMENT CONCRETE DRIVEWAY**

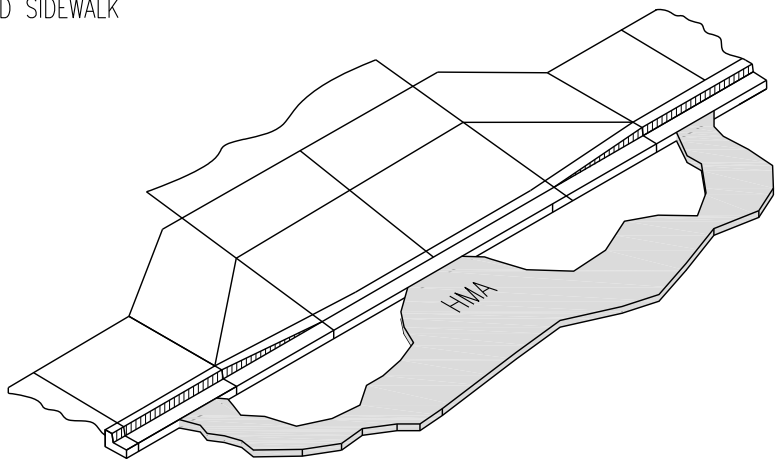


**SECTION A-A**

**LEGEND:** SLOPE

**NOTES:**

1. SEE STANDARD DETAIL ST7 FOR CURB, GUTTER AND SIDEWALK NOTES.
2. WHEN THE DRIVEWAY WIDTH EXCEEDS 15', CONSTRUCT A FULL DEPTH EXPANSION JOINT WITH 3/8" JOINT FILLER ALONG THE DRIVEWAY CENTERLINE. CONSTRUCT EXPANSION JOINTS PARALLEL WITH THE CENTERLINE AS REQUIRED AT 15' MAXIMUM SPACING WHEN DRIVEWAY WIDTHS EXCEED 30'.
3. SEE RMC 12.04.090 AND 12.04.120 FOR ADDITIONAL DETAILS

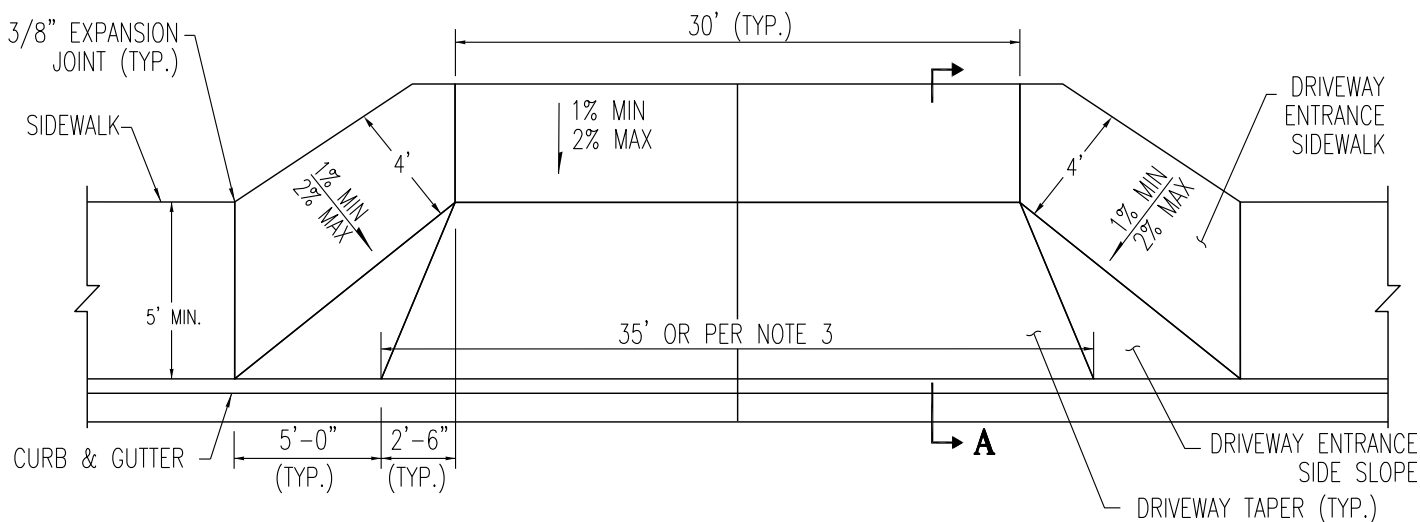


**ISOMETRIC VIEW**

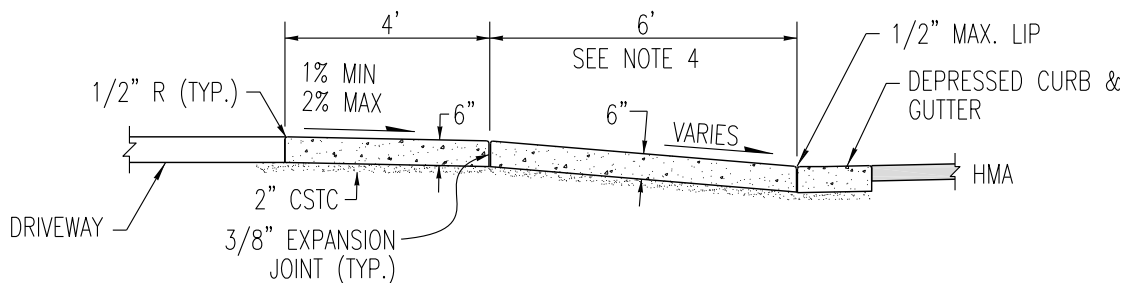


**STANDARD  
RESIDENTIAL  
DRIVEWAY (TYPE 2)**

PUBLIC WORKS ENGINEERING	
APPR. BY: PKR	DATE: 06.19
DRAWN BY: EY	DWG: ST3
CAD FILE: 2013_ST3_06_2019	



**CEMENT CONCRETE DRIVEWAY**

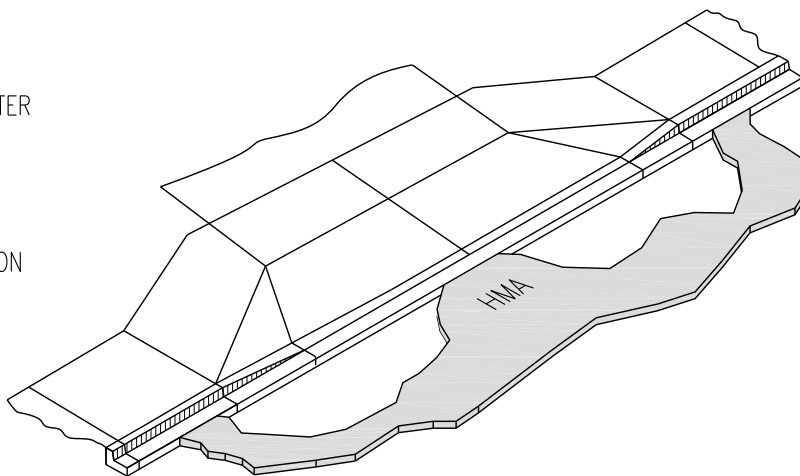


**SECTION A-A**

LEGEND: SLOPE

NOTES:

1. SEE STANDARD DETAIL ST7 FOR CURB, GUTTER AND SIDEWALK NOTES.
2. WHEN THE DRIVEWAY WIDTH EXCEEDS 15', CONSTRUCT A FULL DEPTH EXPANSION JOINT WITH 3/8" JOINT FILLER ALONG THE DRIVEWAY CENTERLINE. CONSTRUCT EXPANSION JOINTS PARALLEL WITH THE CENTERLINE AS REQUIRED AT 15' MAXIMUM SPACING WHEN DRIVEWAY WIDTHS EXCEED 30'.
3. SEE RMC 12.04.095 THRU 12.04.120 FOR ADDITIONAL DETAILS
4. THIS DIMENSION MAY BE REDUCED TO AS LOW AS 4'. REDUCTION REQUIRES VARIANCE REQUEST APPROVED BY CITY ENGINEER.

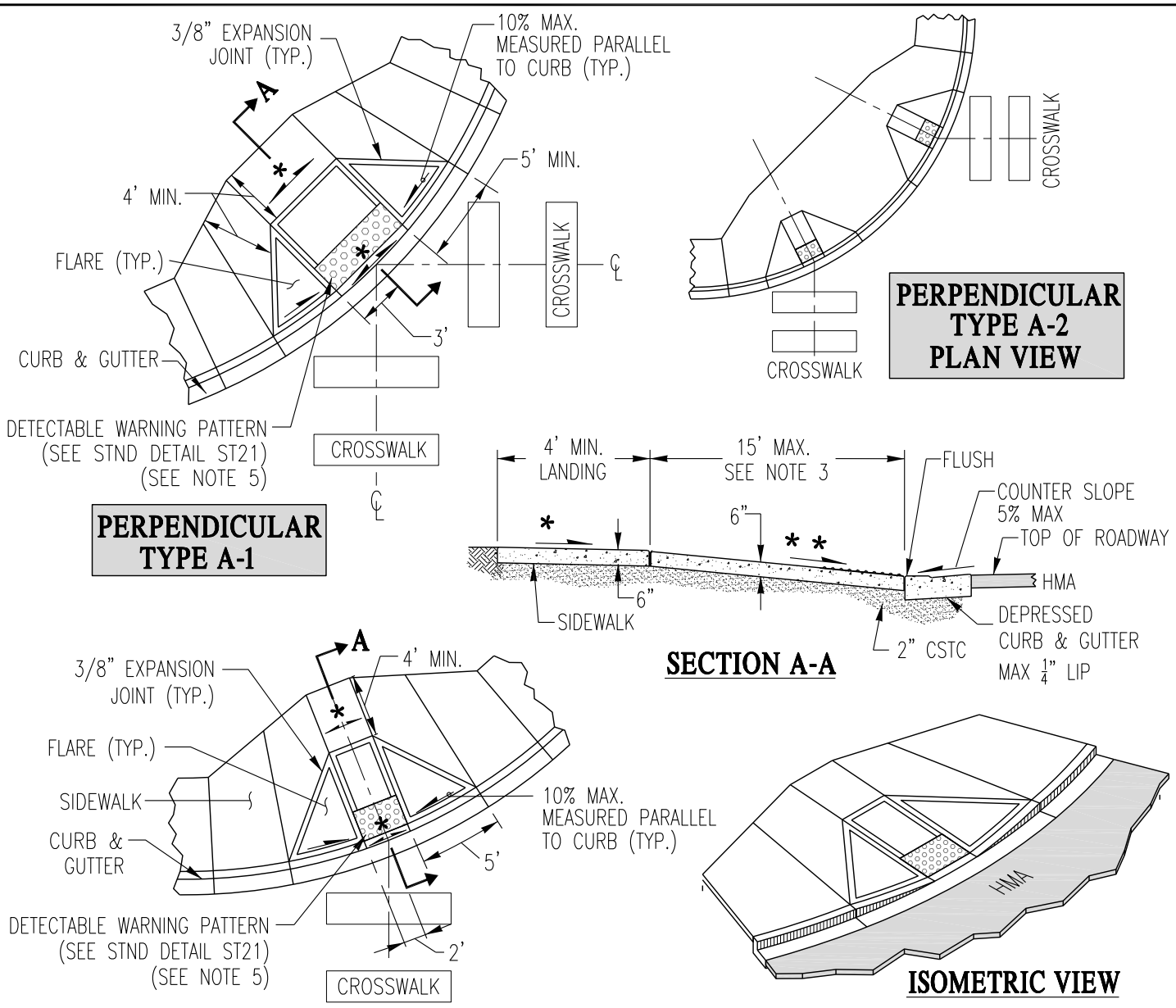


**ISOMETRIC VIEW**



**STANDARD  
NON-RESIDENTIAL  
DRIVEWAY (TYPE 2)**

PUBLIC WORKS ENGINEERING	
APPR. BY: PKR	DATE: 06.19
DRAWN BY: EY	DWG: ST3A
CAD FILE: 2013_ST3A_06_2019	



**PERPENDICULAR TYPE A-1**

**PERPENDICULAR TYPE A-2 PLAN VIEW**

**SECTION A-A**

**ISOMETRIC VIEW**

**PERPENDICULAR TYPE A-2**

**LEGEND:**

- ↔ SLOPE IN EITHER DIRECTION
- \* 1.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (2% MAX)
- \*\* 7.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (8.3% MAX)

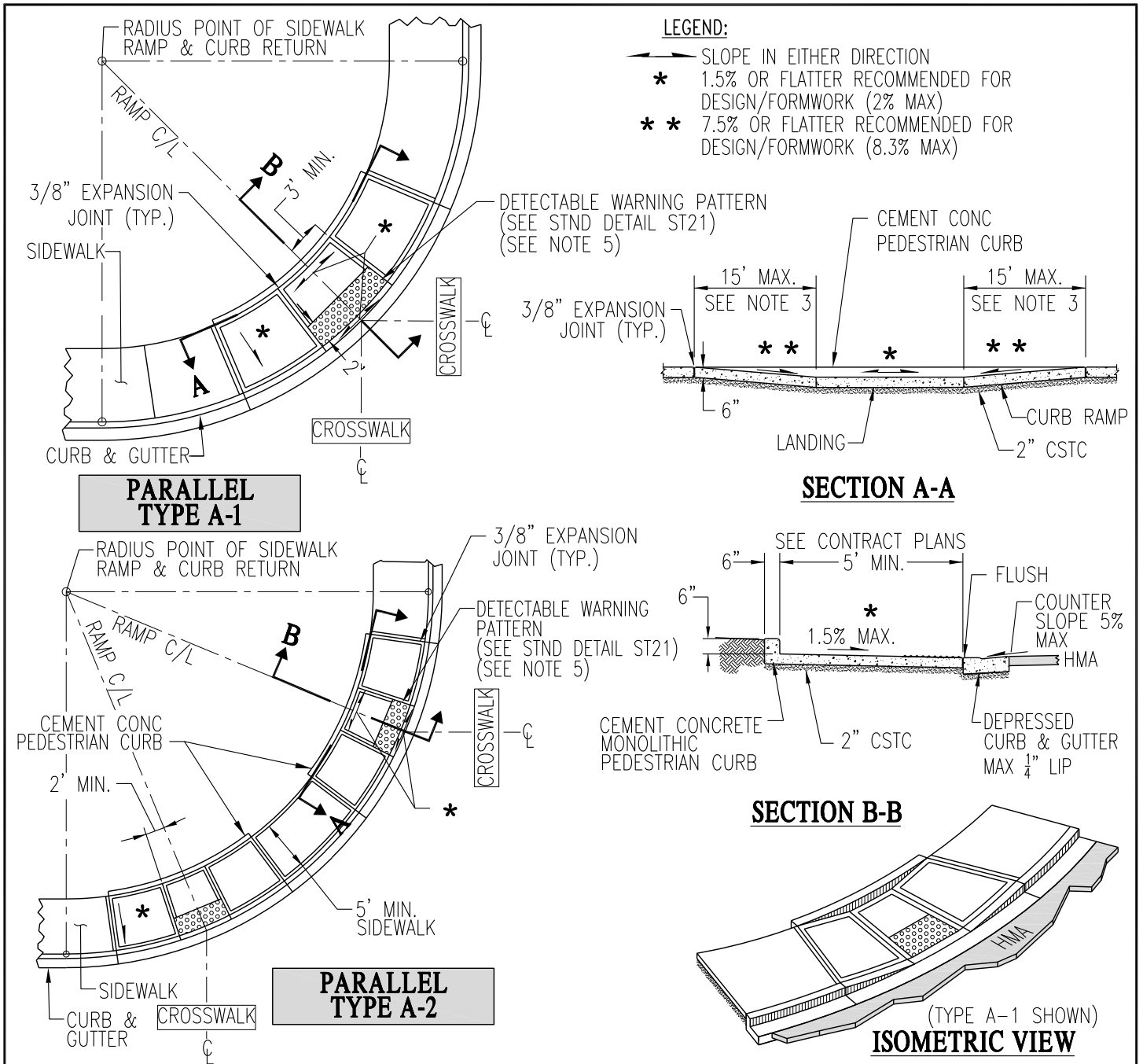
**NOTES:**

1. THE TYPE A-2 ARE THE PREFERRED RAMPS. WHEN IT IS INFEASIBLE TO PROVIDE A RAMP FOR EACH OF THE TWO CROSSWALKS, A SINGLE TYPE A-1 RAMP FOR BOTH CROSSWALKS CAN BE USED WITH THE APPROVAL OF THE ENGINEER.
2. SEE STANDARD DETAIL ST7 FOR CURB, GUTTER AND SIDEWALK NOTES.
3. THE CURB RAMP MAXIMUM RUNNING SLOPE SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15' TO AVOID CHASING THE SLOPE INDEFINITELY WHEN CONNECTING TO STEEP GRADES. WHEN APPLYING THE 15' MAXIMUM LENGTH, THE RUNNING SLOPE OF THE CURB RAMP SHALL BE AS FLAT AS FEASIBLE.
4. DETAILED DESIGN INFORMATION SHALL BE PROVIDED BY THE ENGINEER WITH THE ELEVATIONS PROVIDED FOR ALL FOUR CORNERS OF THE LANDING.
5. CURB SHALL BE FORMED TO PROVIDE A STRAIGHT LINE ALONG THE BACK OF CURB ADJACENT TO THE WARNING SURFACE.



**PERPENDICULAR CURB RAMP TYPE A**

PUBLIC WORKS ENGINEERING	
APPR. BY: PKR	DATE: 02.23
DRAWN BY: JR	DWG: ST4
CAD FILE: 2014_ST4_02_2023	



**NOTES:**

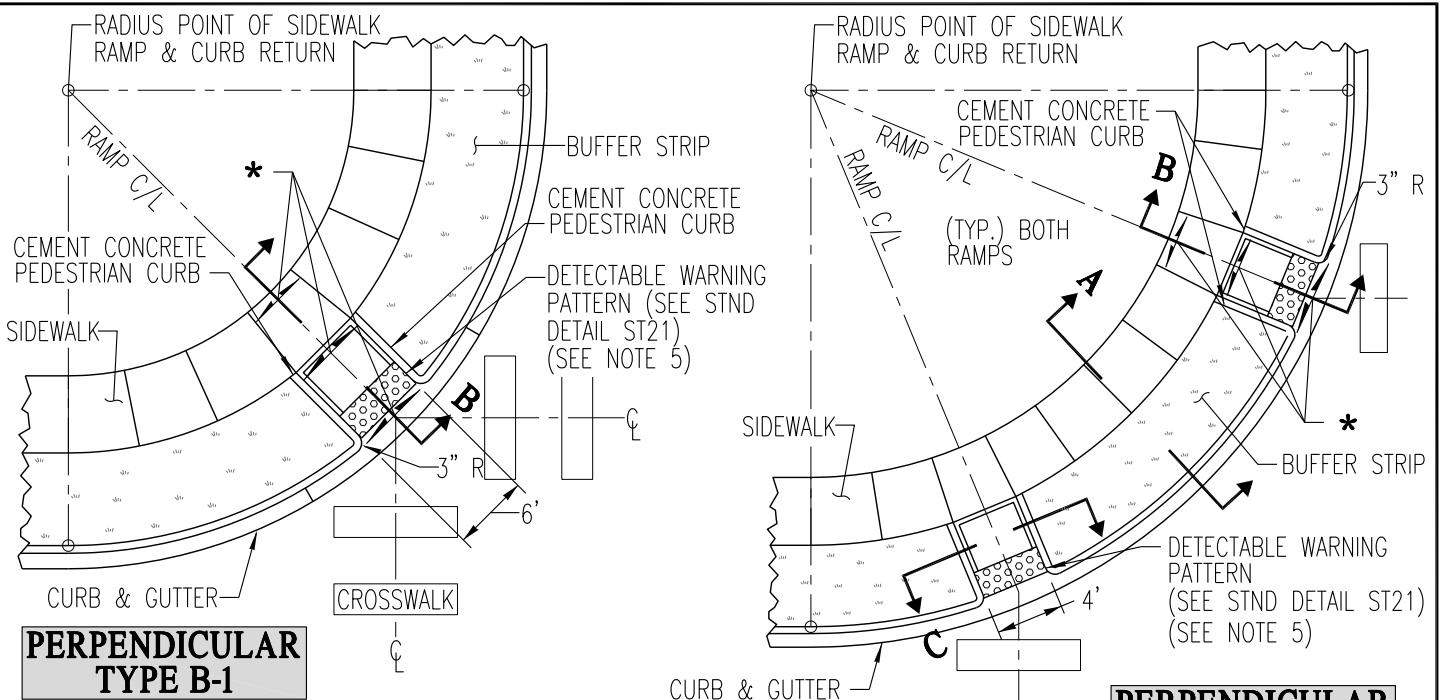
1. THE TYPE A-2 RAMPS ARE THE PREFERRED RAMPS. WHEN IT IS INFEASIBLE TO PROVIDE A RAMP FOR EACH OF THE TWO CROSSWALKS, A SINGLE TYPE A-1 RAMP FOR BOTH CROSSWALKS CAN BE USED WITH THE APPROVAL OF THE ENGINEER.
2. SEE STANDARD DETAIL ST7 FOR CURB, GUTTER AND SIDEWALK NOTES.
3. THE CURB RAMP MAXIMUM RUNNING SLOPE SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15' TO AVOID CHASING THE SLOPE INDEFINITELY WHEN CONNECTING TO STEEP GRADES. WHEN APPLYING THE 15' MAXIMUM LENGTH, THE RUNNING SLOPE OF THE CURB RAMP SHALL BE AS FLAT AS FEASIBLE.
4. DETAILED DESIGN INFORMATION SHALL BE PROVIDED BY ENGINEER ON THE CONSTRUCTION PLANS, INCLUDING DIMENSIONS AND ELEVATIONS AT TOP AND BOTTOM OF LANDING AT BOTH FRONT AND BACK OF SIDEWALK, AS WELL AS CURB RAMP TRANSITION LENGTHS. MINIMUM CURB LENGTH TRANSITION IS 5'.
5. CURB SHALL BE FORMED TO PROVIDE A STRAIGHT LINE ALONG THE BACK OF CURB ADJACENT TO THE WARNING PATTERN.



**PARALLEL  
CURB RAMP  
TYPE A**

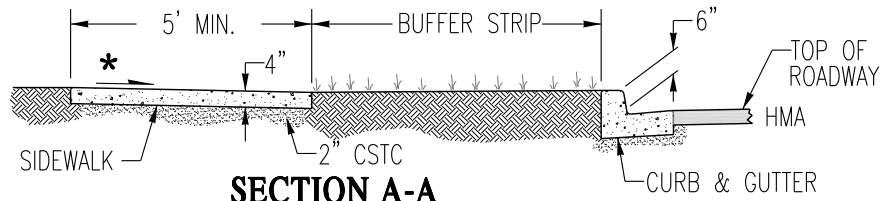
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APPR. BY: PKR	DATE: 02.23
DRAWN BY: JR	DWG: ST5
CAD FILE: 2013_ST5_02_2023	



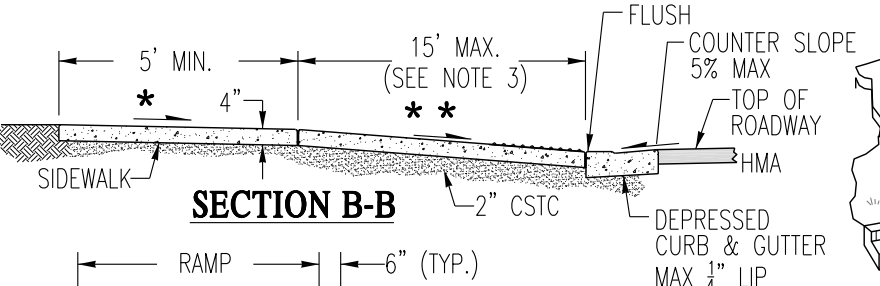


**PERPENDICULAR  
TYPE B-1**

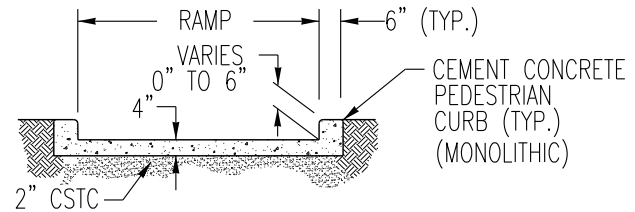
**PERPENDICULAR  
TYPE B-2**



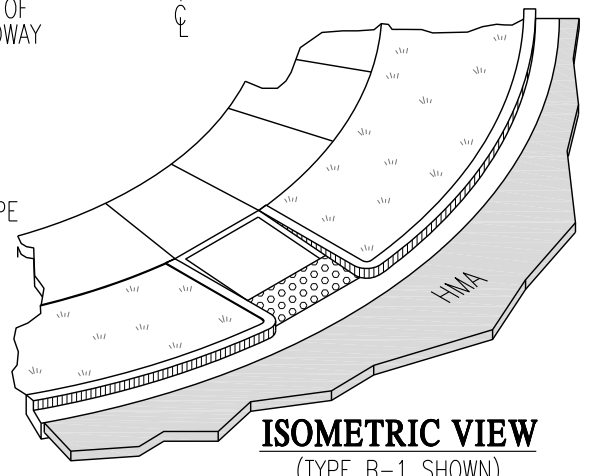
**SECTION A-A**



**SECTION B-B**



**SECTION C-C**



**ISOMETRIC VIEW  
(TYPE B-1 SHOWN)**

**LEGEND:**  
 ← → SLOPE IN EITHER DIRECTION  
 \* 1.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (2% MAX)  
 \*\* 7.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (8.3% MAX)

**NOTES:**

1. THE TYPE B-2 RAMP ARE THE PREFERRED RAMPS. WHEN IT IS INFEASIBLE TO PROVIDE A RAMP FOR EACH OF THE TWO CROSSWALKS, A SINGLE TYPE B-1 RAMP FOR BOTH CROSSWALKS CAN BE USED WITH THE APPROVAL OF THE ENGINEER.
2. SEE STANDARD DETAIL ST7 FOR CURB, GUTTER AND SIDEWALK NOTES.
3. THE CURB RAMP MAXIMUM RUNNING SLOPE SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15' TO AVOID CHASING THE CURB RAMP MAXIMUM RUNNING SLOPE SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15' TO AVOID CHASING RUNNING SLOPE OF THE CURB RAMP SHALL BE AS FLAT AS FEASIBLE.
4. DETAILED DESIGN INFORMATION SHALL BE PROVIDED BY THE ENGINEER WITH THE ELEVATIONS PROVIDED FOR ALL FOUR CORNERS OF THE LANDING AND RAMP LENGTH.
5. CURB SHALL BE FORMED TO PROVIDE A STRAIGHT LINE ALONG THE BACK OF CURB ADJACENT TO THE WARNING PATTERN.



**PERPENDICULAR  
CURB RAMP  
TYPE B**

PUBLIC WORKS ENGINEERING	
APPR. BY: PKR	DATE: 02.23
DRAWN BY: JR	DWG: ST6
CAD FILE: 2013_ST6_02_2023	

## CONSTRUCTION NOTES FOR PEDESTRIAN FACILITIES

1. ASPHALT PATCHING – SAWCUT A MINIMUM OF 24" OF ASPHALT BEYOND THE FACE OF NEW GUTTER, REMOVE ALL DEBRIS AND ADD CRUSHED SURFACE TOP COURSE (CSTC) AS NEEDED. COMPACT AREA, PLACE HMA AND COMPACT AS REQUIRED.
2. CONCRETE SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 3000 PSI FOR SIDEWALKS. CONCRETE FOR CURB, GUTTER AND DRIVEWAYS SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 4000 PSI.
3. CONTRACTOR SHALL CALL FOR CITY INSPECTION PRIOR TO CUTTING AND/OR REMOVING CURB, GUTTER, AND SIDEWALK. THE INSPECTOR WILL MARK AREA TO BE CUT OR REMOVED. CONTRACTOR'S PRESENCE IS ADVISABLE.
4. CONTRACTOR SHALL CALL FOR INSPECTION OF ALL FORMS PRIOR TO POURING CONCRETE FOR CURB, GUTTER, SIDEWALK AND DRIVEWAYS. ALL EXPANSION JOINTS SHALL BE IN PLACE AT TIME OF INSPECTION. ("WET SET" MASTIC IS NOT ALLOWED)
5. THE INSPECTOR SHALL CHECK ALL JOINT LOCATIONS. ALL EXPANSION JOINTS SHALL BE MARKED WITH AN "M".
6. CONTRACTOR SHALL NOT POUR ANY CONCRETE UNTIL ALL JOINTS HAVE BEEN CHECKED AND GIVEN VERBAL OR WRITTEN APPROVAL BY INSPECTOR.
7. THE FACE OF CURB SHALL BE STAMPED AT ALL UTILITY CROSSINGS, MAIN LINES AND SERVICE LINES AS FOLLOWS: "S" –SANITARY SEWER, "W" –WATER, "I" –IRRIGATION, "C" –CONDUITS
8. CURB, GUTTER, AND SIDEWALK SURFACES SHALL HAVE A LIGHT BROOM FINISH. SIDEWALK SHALL BE BROOMED PERPENDICULAR TO CURB LINE IN A UNIFORM AND CONSISTENT MANNER.
9. A MINIMUM OF 2" OF CSTC SHALL BE PLACED AND COMPACTED UNDER ALL CURB, GUTTER AND SIDEWALK.
10. JOINT SPACING SHALL BE NO LESS THAN 2.5' AND NO GREATER THAN 5'.
11. SIDEWALKS SHALL BE A MINIMUM OF 5 FEET WIDE. IN COMMERCIAL ZONED C-2, C-3, AND CBD AREAS THE SIDEWALKS SHALL BE 8 FEET WIDE.
12. WHEN UTILITY METERS, VAULTS, TRANSFORMERS, ETC. EXIST IN THE AREA BETWEEN THE LOT LINE AND THE DRIVEWAY, THE DRIVEWAY MUST BE CONSTRUCTED AT LEAST 10' FROM THE LOT LINE.
13. IF A CONCRETE DRIVEWAY IS TO BE EXTENDED PAST THE R/W LINE A 3/8" MASTIC EXPANSION JOINT SHALL BE INSTALLED FULL LENGTH OF DRIVEWAY AND FULL DEPTH OF DRIVEWAY AT THE BACK OF SIDEWALK. DRIVEWAY SHALL LINE UP WITH THE BOTTOM OF THE DRIVEWAY TRANSITIONS IN THE CURB AND GUTTER.
14. MAINTAIN 4' MINIMUM CLEARANCE FROM ANY OBSTRUCTION ON SIDEWALK AND SIDEWALK RAMP.
15. AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES, OR OTHER OBSTRUCTIONS IN FRONT OF DRIVEWAY ENTRANCES.
16. AT NO TIME SHALL ANY SLOPES EXCEED CURRENT ADA STANDARDS.



### CURB, GUTTER & SIDEWALK NOTES

PUBLIC WORKS ENGINEERING

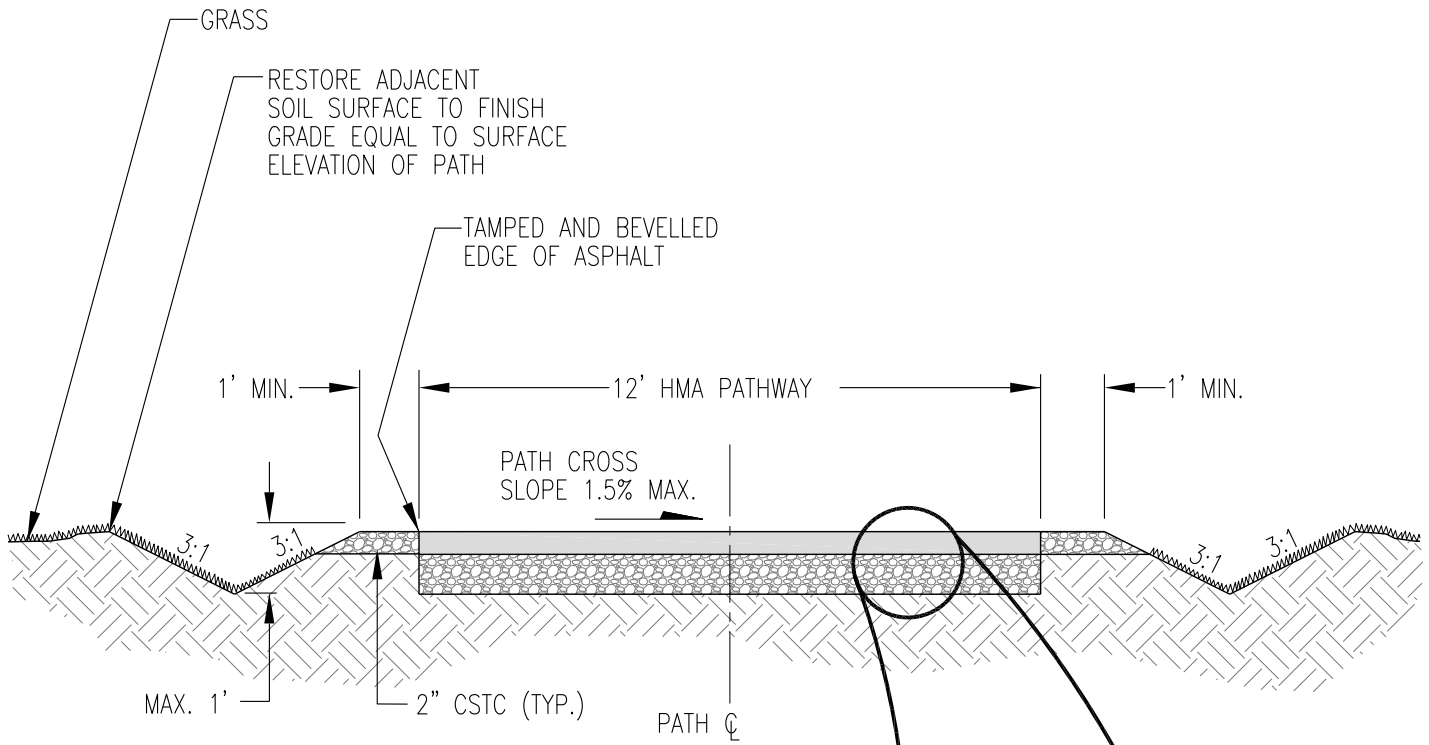
APPR. BY: PKR

DATE: 09.13

DRAWN BY: LD

DWG: ST7

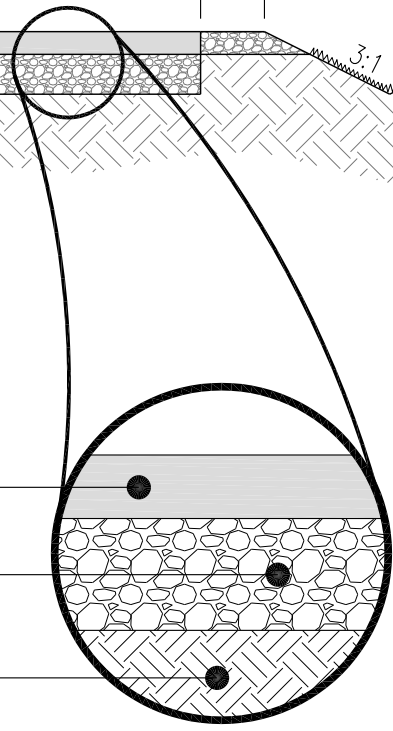
CAD FILE: 2013\_ST7\_09\_2013



2" COMMERCIAL HMA  
OR 4" CONCRETE FOR  
MIDBLOCK PEDESTRIAN WALKWAY  
APPLY SOIL RESIDUAL HERBICIDE  
PRIOR TO PAVING

4" MIN. CSTC

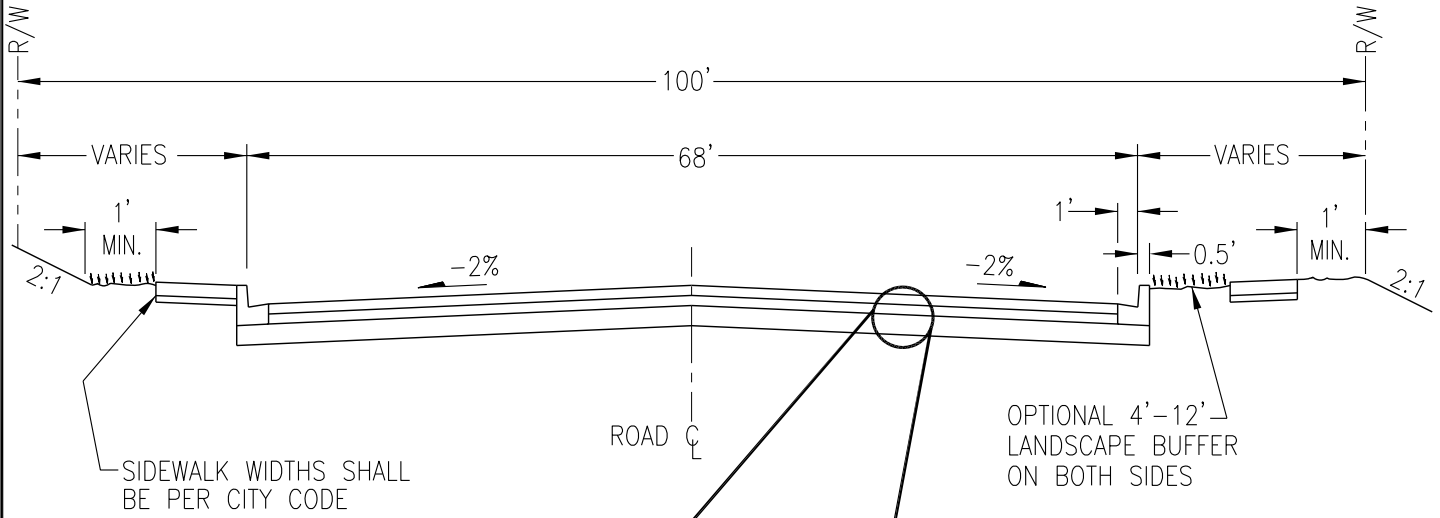
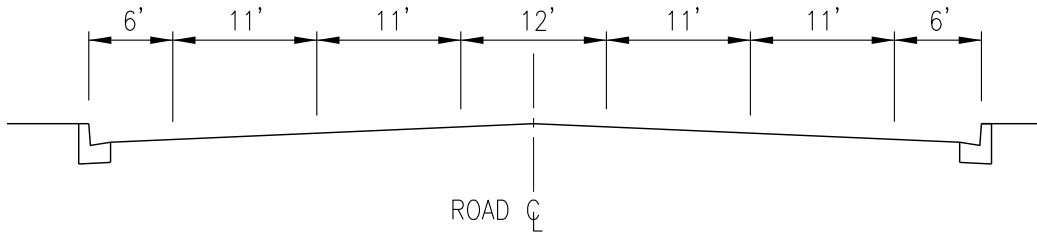
COMPACTED SUBGRADE



# BIKE/PED PATH

PUBLIC WORKS ENGINEERING	
APPR. BY: PKR	DATE: 09.13
DRAWN BY: LD	DWG: ST8
CAD FILE: 2013_ST8_09_2013	

TYPICAL LANE LAYOUT

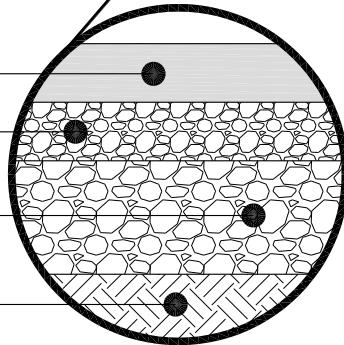


4" HMA CL 1/2", PG 64-28  
 APPLY SOIL RESIDUAL HERBICIDE  
 PRIOR TO PAVING

2" CSTC

8" CSBC

COMPACTED SUBGRADE



PRINCIPAL  
 ARTERIAL

PUBLIC WORKS ENGINEERING

APPR. BY: SAW

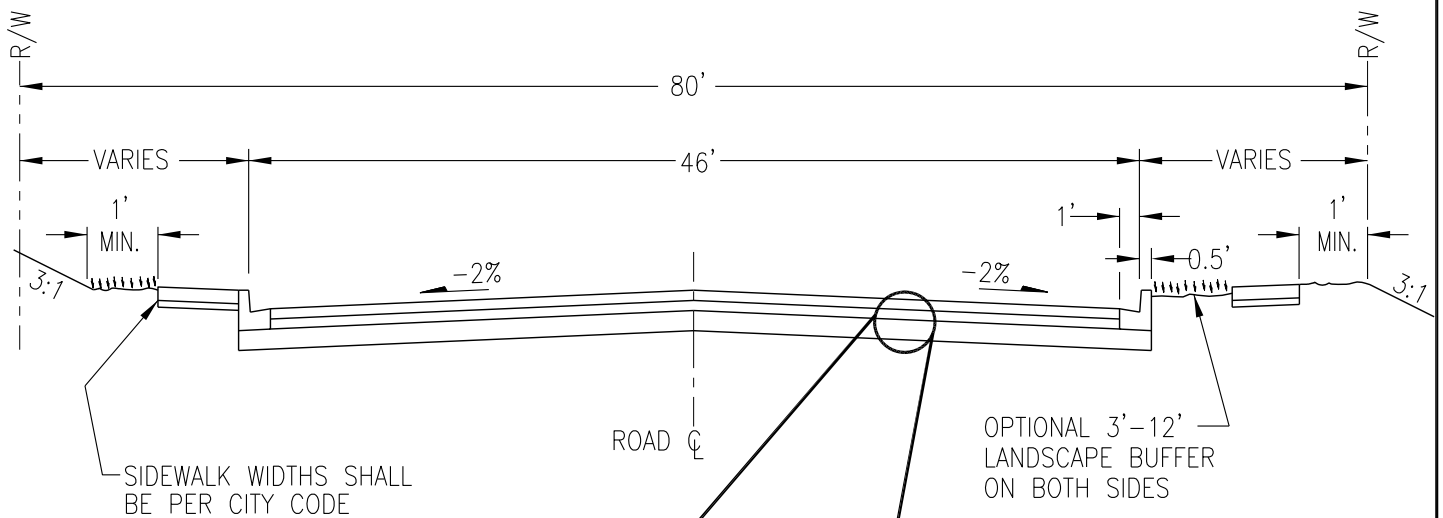
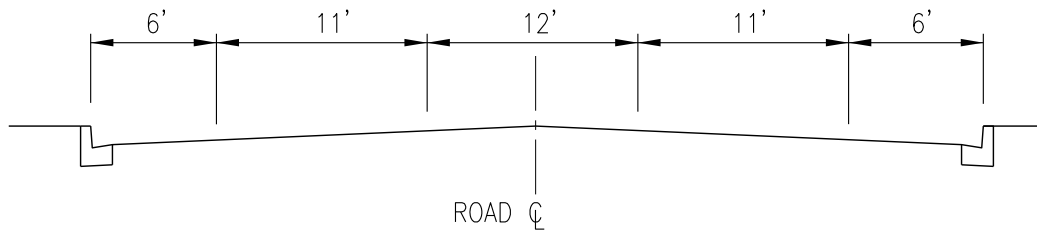
DATE: 01.24

DRAWN BY: JLR

DWG: ST9

CAD FILE: 2013\_ST9\_01\_2024

TYPICAL LANE LAYOUT

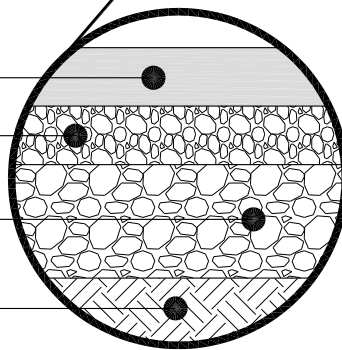


4" HMA CL 1/2", PG 64-28  
 APPLY SOIL RESIDUAL HERBICIDE  
 PRIOR TO PAVING

2" CSTC

8" CSBC

COMPACTED SUBGRADE



OPTIONAL 3'-12'  
 LANDSCAPE BUFFER  
 ON BOTH SIDES

NOTES:

1. WHEN A MINOR ARTERIAL REQUIRES A 5-LANE CROSS SECTION AS DETERMINED BY THE PUBLIC WORKS DIRECTOR, THE CROSS SECTION DRAWING FOR PRINCIPAL ARTERIAL (ST9) SHALL BE USED.



MINOR  
 ARTERIAL

PUBLIC WORKS ENGINEERING

APPR. BY: SAW

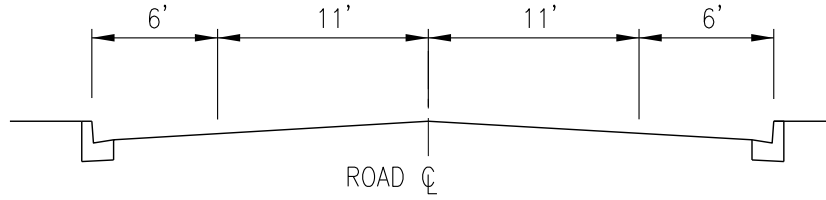
DATE: 01.24

DRAWN BY: JLR

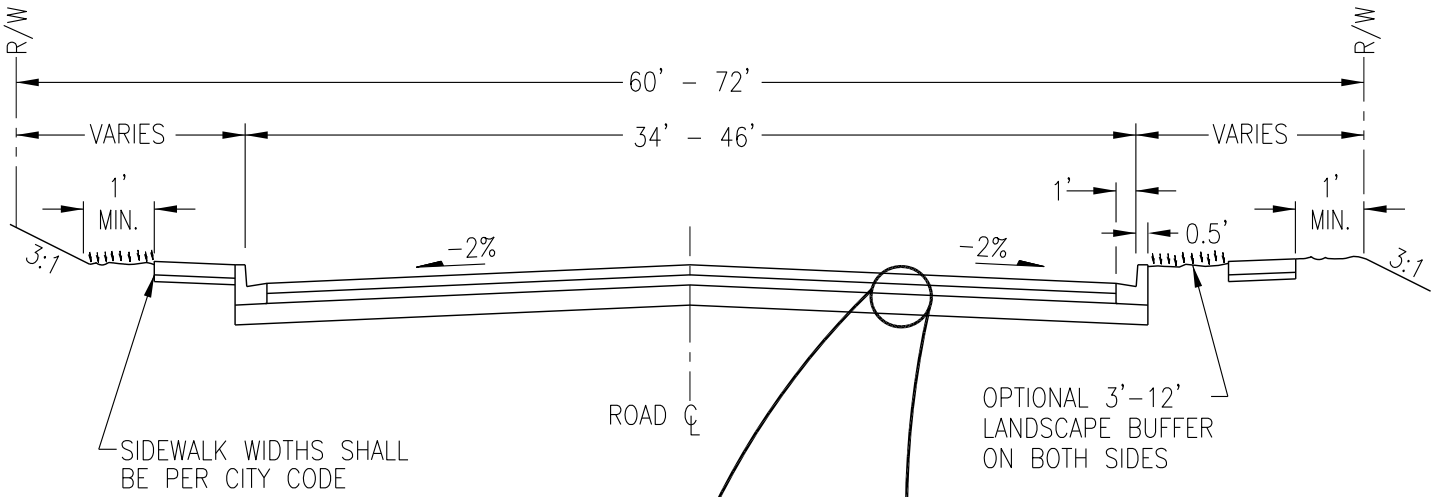
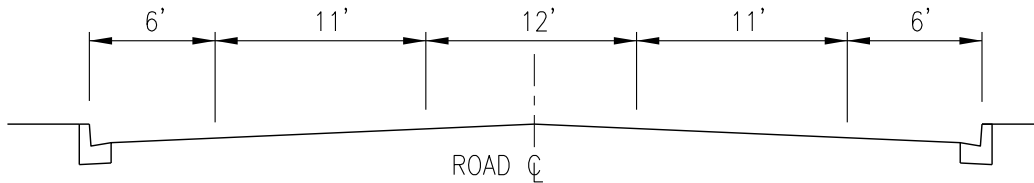
DWG: ST10

CAD FILE: 2013\_ST10\_01\_2024

LANE OPTION 1



LANE OPTION 2

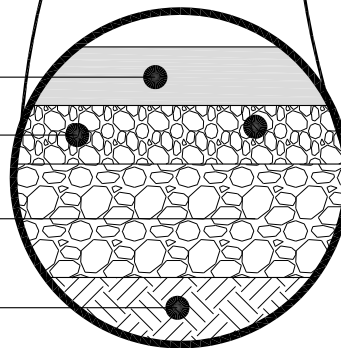


4" HMA CL 1/2", PG 64-28  
APPLY SOIL RESIDUAL HERBICIDE  
PRIOR TO PAVING

2" CSTC

8" CSBC

COMPACTED SUBGRADE



NOTES:

1. OPTION USED WILL BE DETERMINED BY CITY ENGINEER; DEPENDENT UPON FIELD CONDITIONS.
2. A PARKING OPTION MAY BE ALLOWED ON ONE OR BOTH SIDES BY ADDING 8' OUTSIDE OF THE BIKE LANE.



MAJOR  
COLLECTOR  
(Arterial Collector)

PUBLIC WORKS ENGINEERING

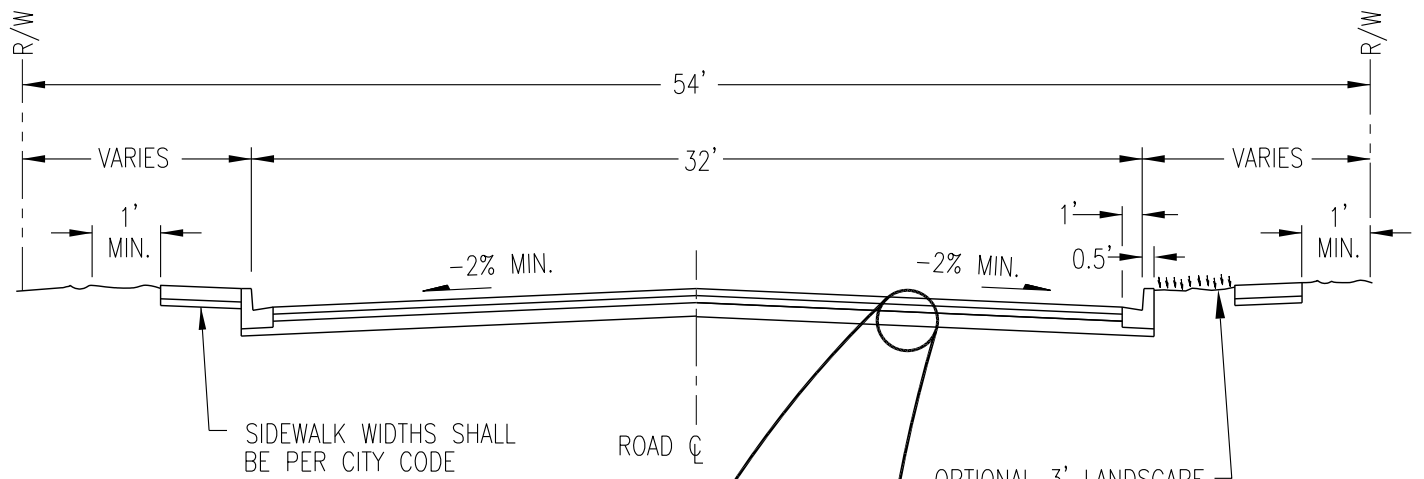
APPR. BY: SAW

DATE: 01.24

DRAWN BY: JLR

DWG: ST11

CAD FILE: 2013\_ST11\_01\_2024



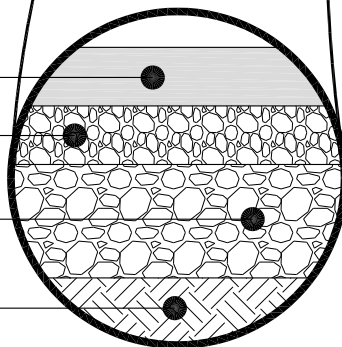
OPTIONAL 3' LANDSCAPE BUFFER AREA OR 6' IF STREET TREES ARE USED

2" HMA CL 3/8" PG 64-28  
APPLY SOIL RESIDUAL HERBICIDE  
PRIOR TO PAVING

2" CSTC

6" CSBC

COMPACTED SUBGRADE



MINOR (NEIGHBORHOOD)  
COLLECTOR  
and LOCAL STREET

PUBLIC WORKS ENGINEERING

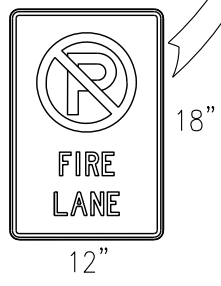
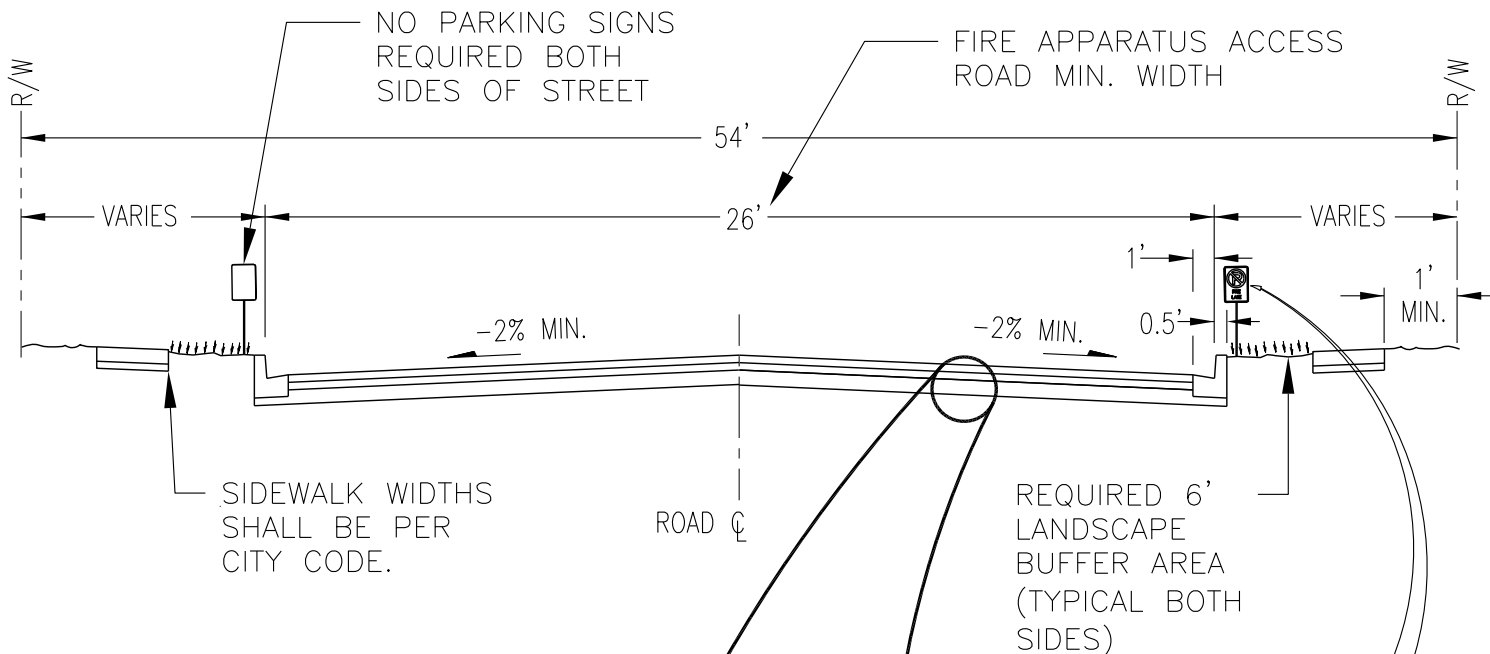
APPR. BY: SAW

DATE: 01.24

DRAWN BY: JLR

DWG: ST13

CAD FILE: 2013\_ST13\_01\_2024



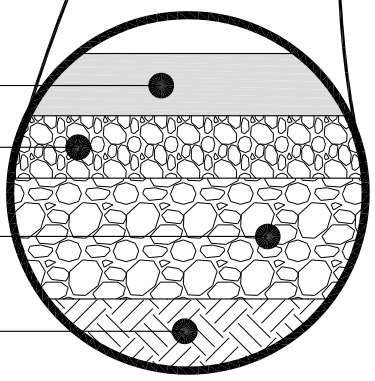
R8-3(FL)  
SEE TR3-6  
FOR SIGN  
DETAILS &  
INSTALLATION

2" HMA CL 3/8" PG 64-28  
APPLY SOIL RESIDUAL HERBICIDE  
PRIOR TO PAVING

2" CSTC

6" CSBC

COMPACTED SUBGRADE



PERMISSIVE USE CASE: USE OF THIS NARROW STREET SECTION IS PERMITTED ONLY IN R-2S AND R-3 ZONED BLOCKS WHERE AVERAGE LOT WIDTH IS 40 FT OR LESS AND ADEQUATE OFF-STREET PARKING IS PROVIDED, OR AS APPROVED BY THE CITY ENGINEER.



# NARROW LOCAL STREET

PUBLIC WORKS ENGINEERING

APPR. BY: SAW

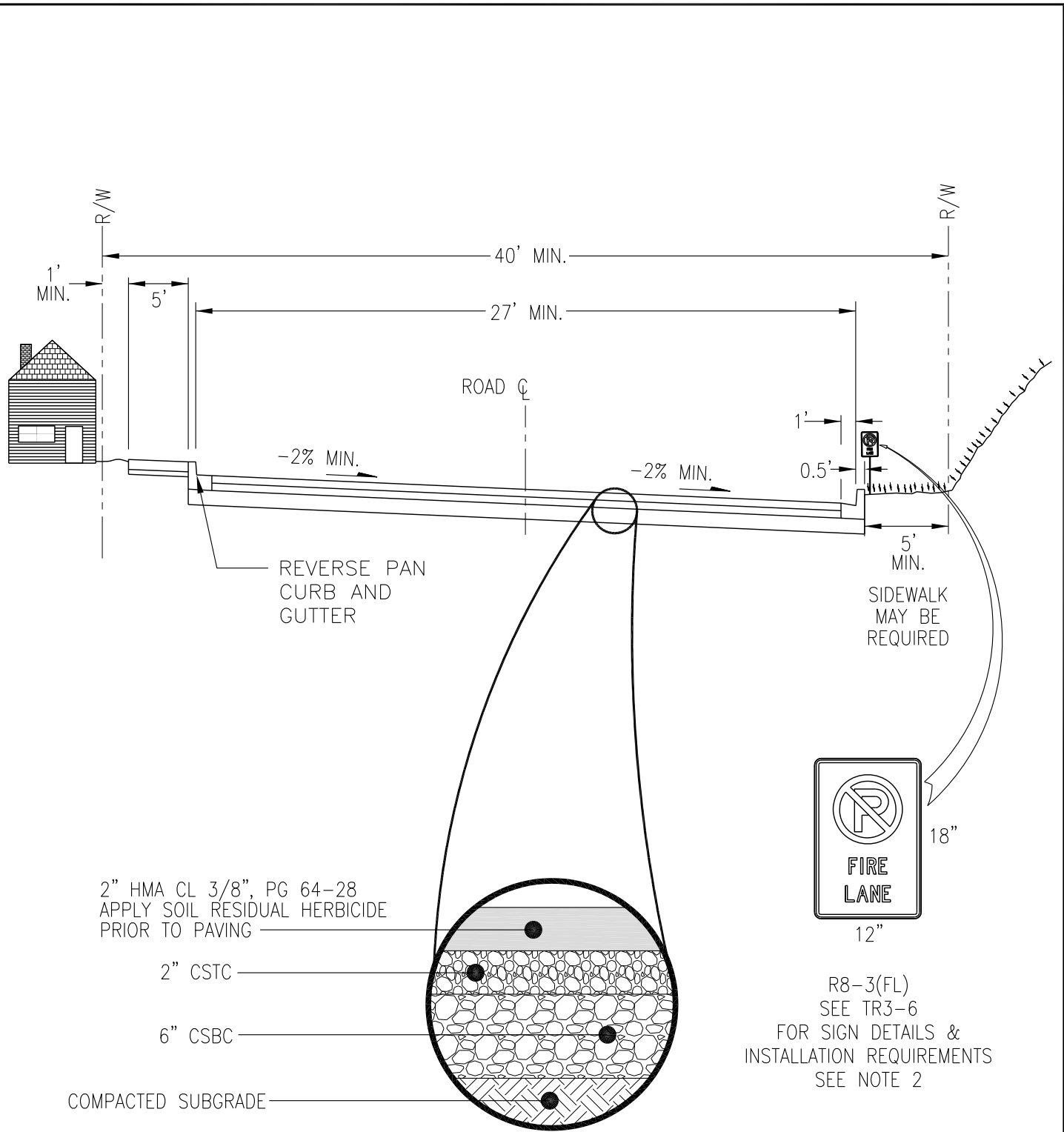
DATE: 01.24

DRAWN BY: JLR

DWG: ST13A

CAD FILE: 2013\_ST13A\_01\_2024





\*CITY ENGINEER SHALL APPROVE SLOPING OF STREET TO MATCH DIRECTION OF EXISTING GRADE.

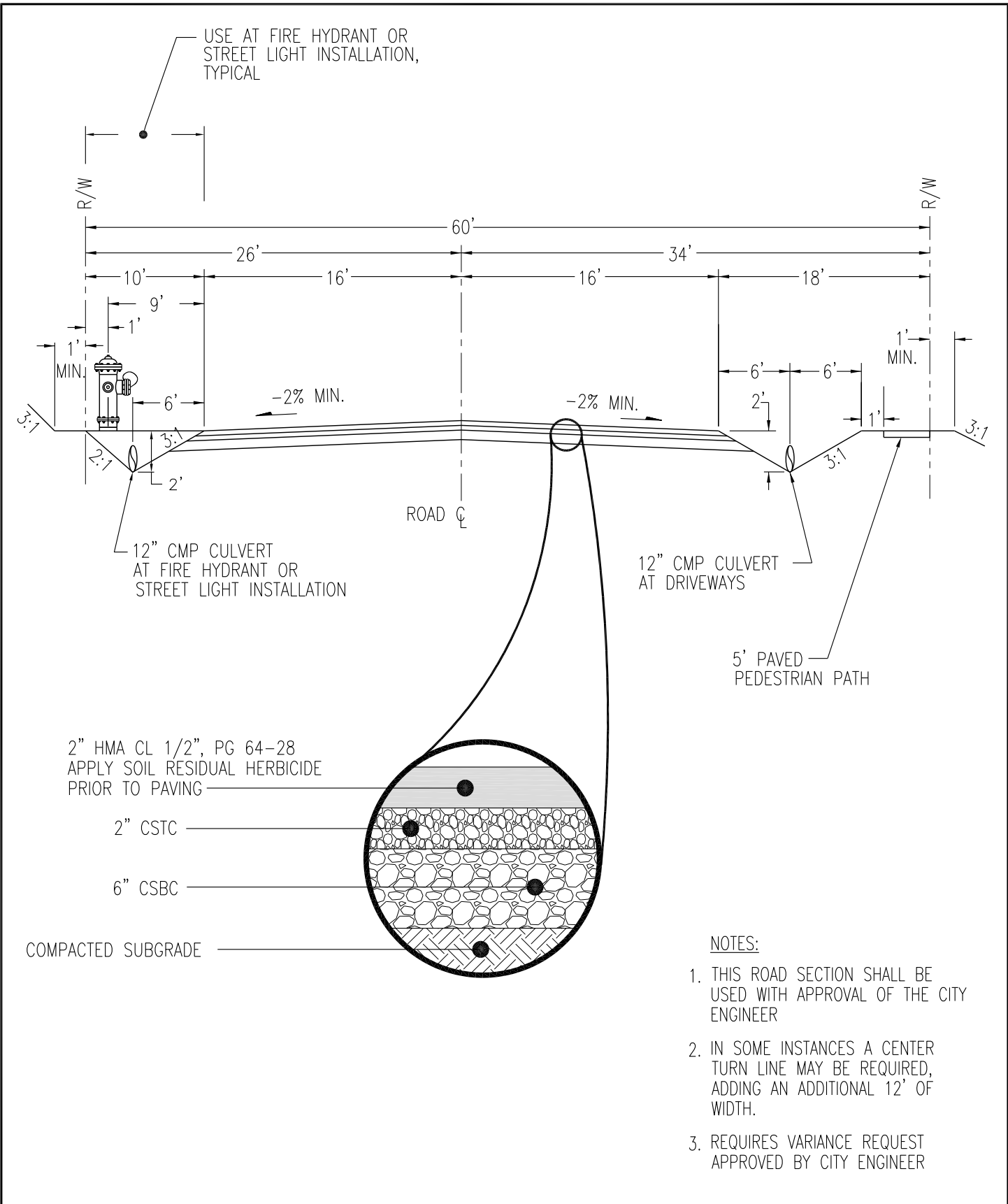
**NOTES:**

1. THIS ROAD SECTION SHALL ONLY BE USED WITH APPROVAL OF THE CITY ENGINEER.
2. IF DIRECTED PROVIDE NO PARKING THIS SIDE OF STREET. [R8-3(TSOS)]



# LOCAL STREET (SINGLE FRONTAGE)

PUBLIC WORKS ENGINEERING	
APPR. BY: SAW	DATE: 01.24
DRAWN BY: JLR	DWG: ST14
CAD FILE: 2013_ST14_01_2024	



# RURAL STREET

PUBLIC WORKS ENGINEERING

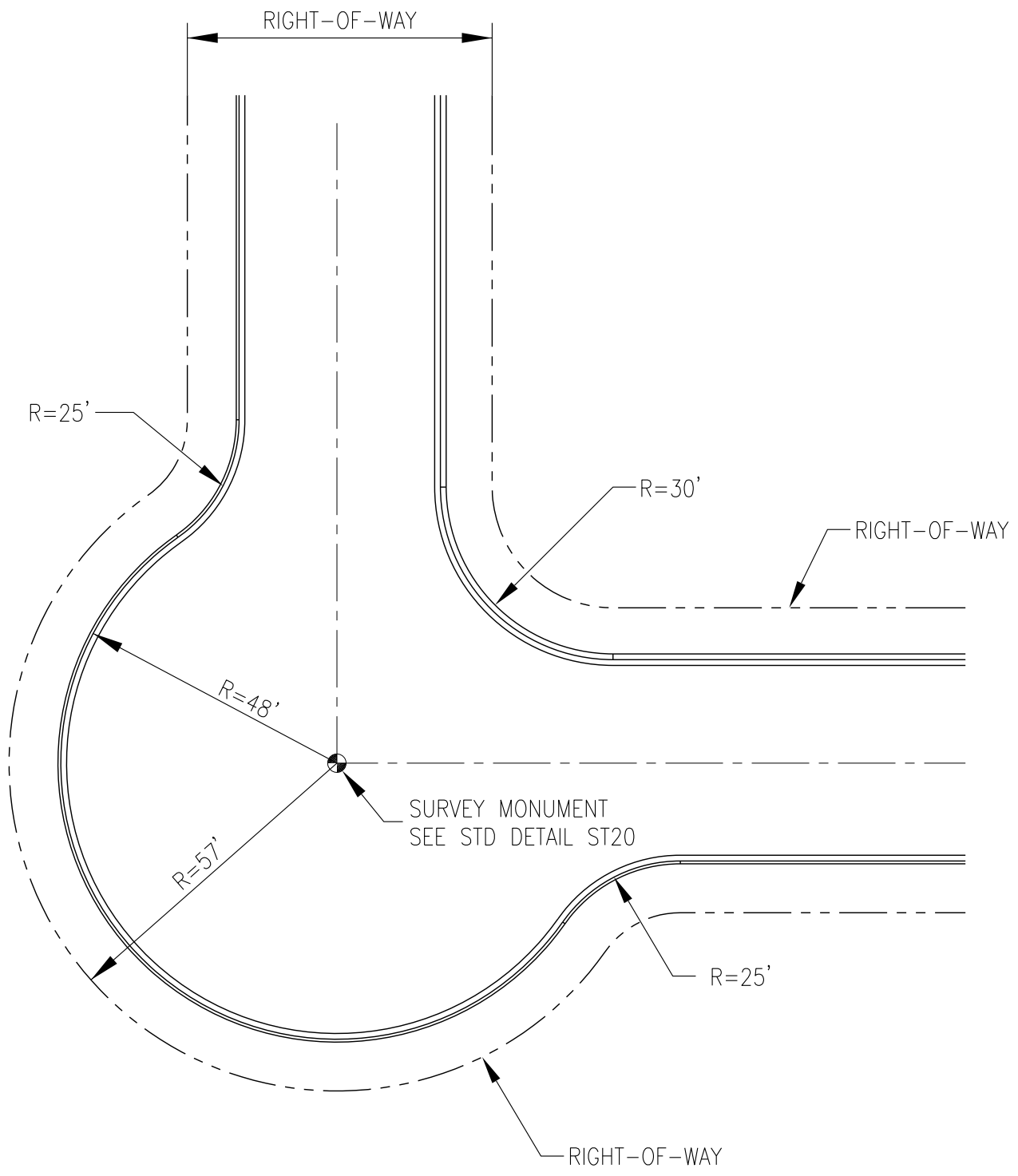
APPR. BY: PKR

DATE: 06.19

DRAWN BY: LD

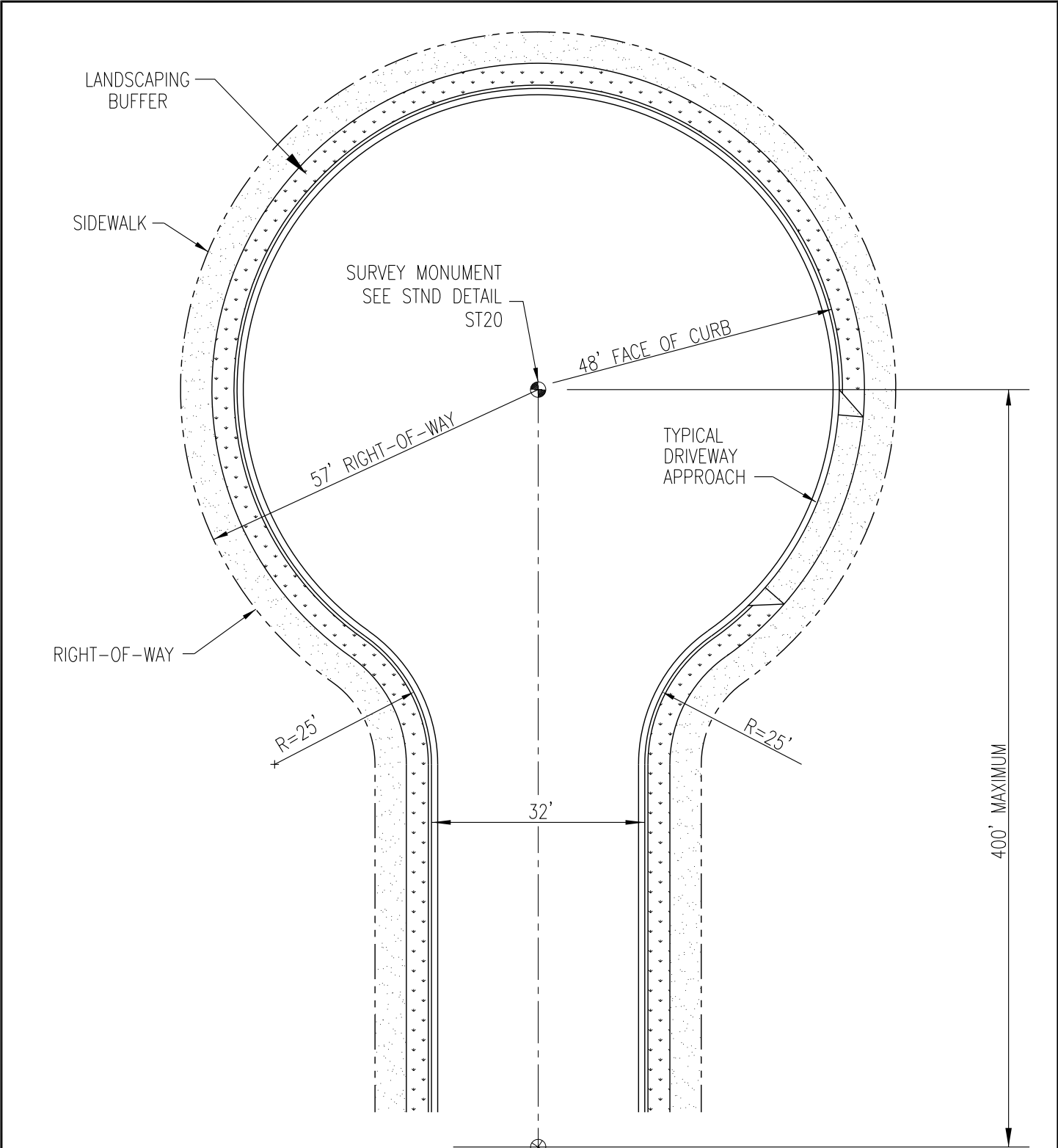
DWG: ST15

CAD FILE: 2013\_ST15\_06\_2019



LOCAL STREET  
RIGHT ANGLE  
INTERSECTION

CIVIL & UTILITY ENGINEERING	
APPR. BY: PKR	DATE: 06.19
DRAWN BY: LD	DWG: ST16
CAD FILE: 2012_ST16_06_2019	



**NOTES:**

1. CITY ENGINEER SHALL APPROVE DRIVEWAY TYPE AND LOCATION IN CUL-DE-SAC.
2. CITY ENGINEER SHALL APPROVE DEPRESSED CURB.
3. LANDSCAPING BUFFER AND SIDEWALK WIDTHS BASED ON ZONING REQUIREMENTS.



**STANDARD  
CUL-DE-SAC  
BULB**

PUBLIC WORKS ENGINEERING	
APPR. BY: SAW	DATE: 01.24
DRAWN BY: JLR	DWG: ST17
CAD FILE: 2012_ST17_01_2024	

LANDSCAPING  
BUFFER

SIDEWALK

48' FACE OF CURB

59' RIGHT-OF-WAY

32' MIN.

STANDARD MONUMENT  
SEE STND. DETAIL ST20

TYPICAL  
DRIVEWAY  
APPROACH

R=25'

RIGHT-OF-WAY

32'

400' MAXIMUM

27' MIN.

NOTES:

- 1. CITY ENGINEER SHALL APPROVE DEPRESSED CURB.
- 2. CITY ENGINEER SHALL APPROVE DRIVEWAY LOCATION IN CUL-DE-SAC
- 3. LANDSCAPING BUFFER AND SIDEWALK WIDTHS BASED ON ZONING REQUIREMENTS

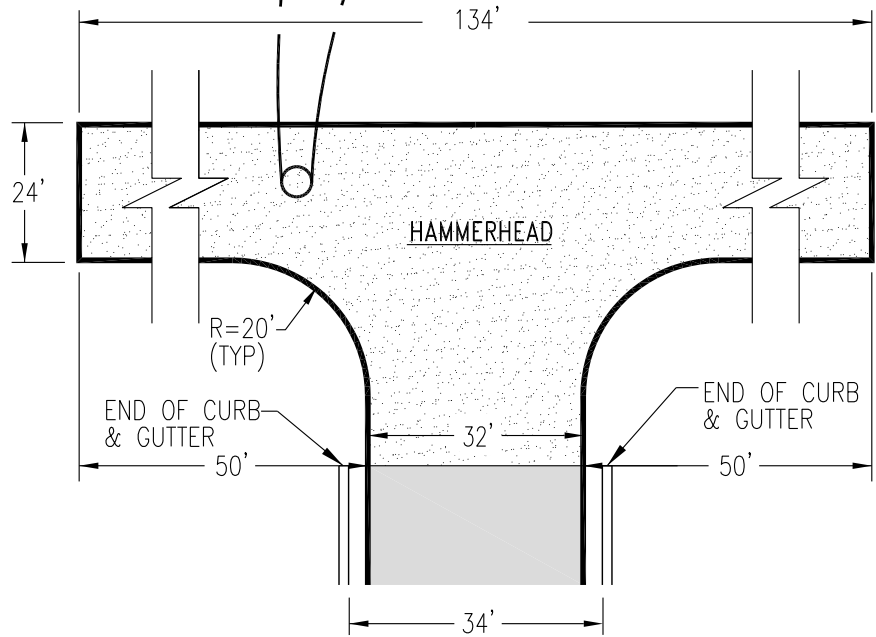
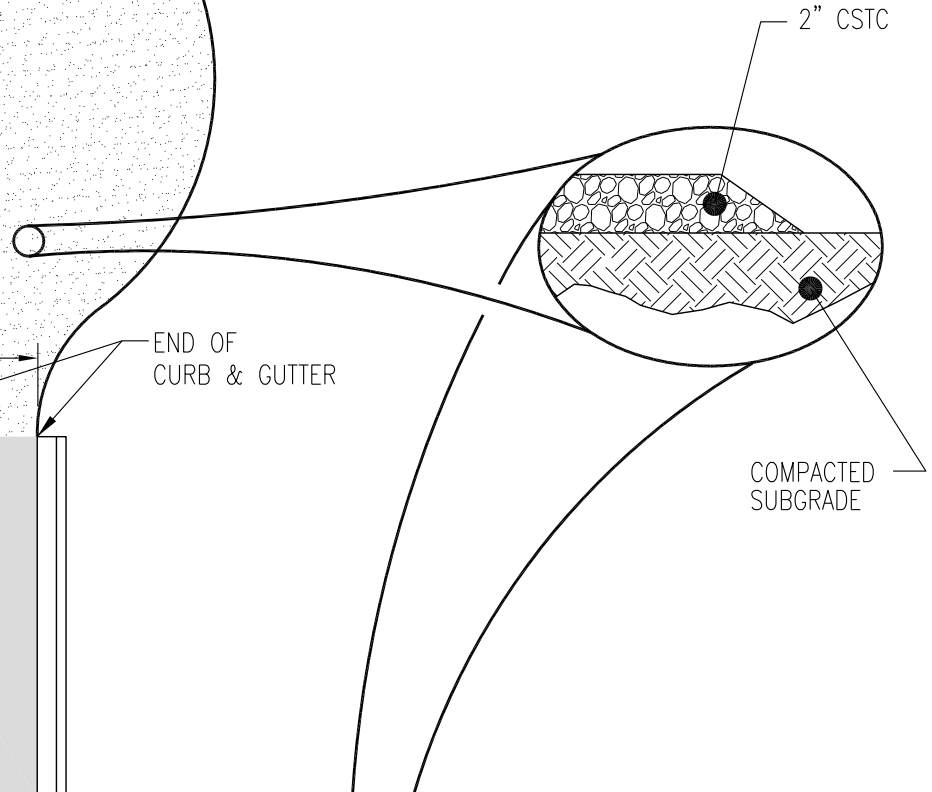
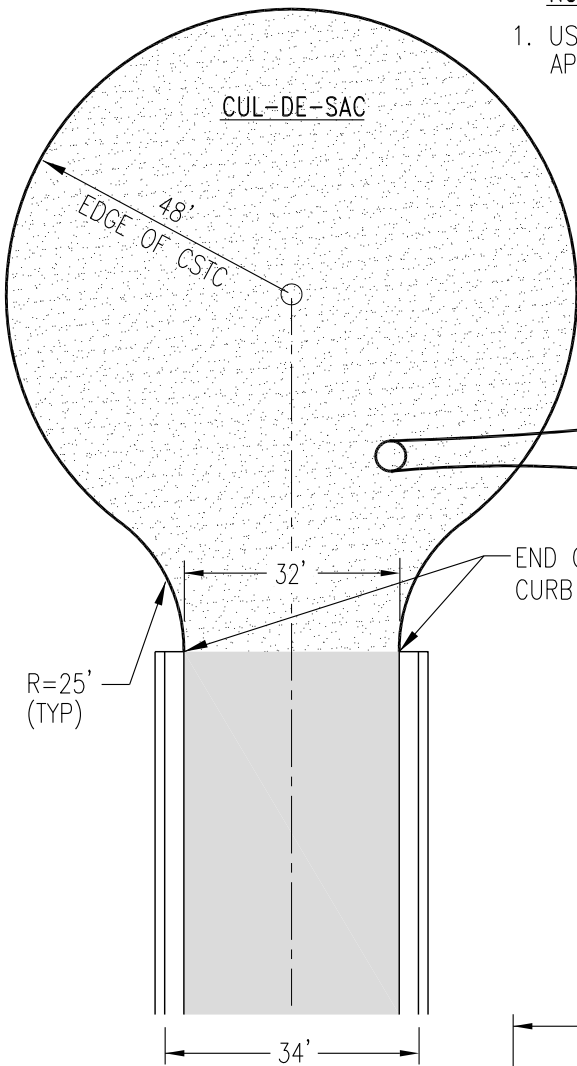


# STANDARD OFFSET CUL-DE-SAC

PUBLIC WORKS ENGINEERING	
APPR. BY: SAW	DATE: 01.24
DRAWN BY: JLR	DWG: ST18
CAD FILE: 2012_ST18_01_2024	

**NOTE:**

1. USE OF THIS TURN AROUND AT ANY SPECIFIC SITE MUST BE APPROVED BY THE CITY FIRE MARSHALL AND THE CITY ENGINEER.



**TEMPORARY  
TURN AROUND**

PUBLIC WORKS ENGINEERING

APPR. BY: PKR

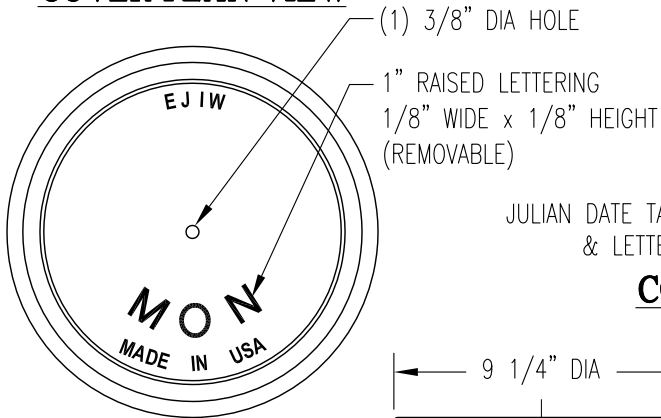
DATE: 06.19

DRAWN BY: LD

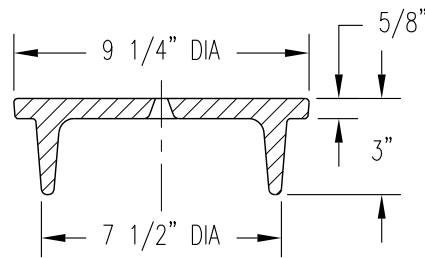
DWG: ST19

CAD FILE: 2012\_ST19\_06\_2019

**COVER-PLAN VIEW**

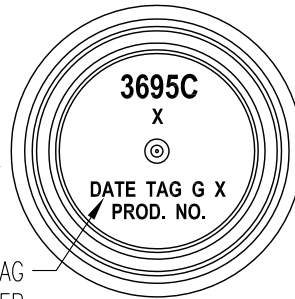


**SECTION-COVER**

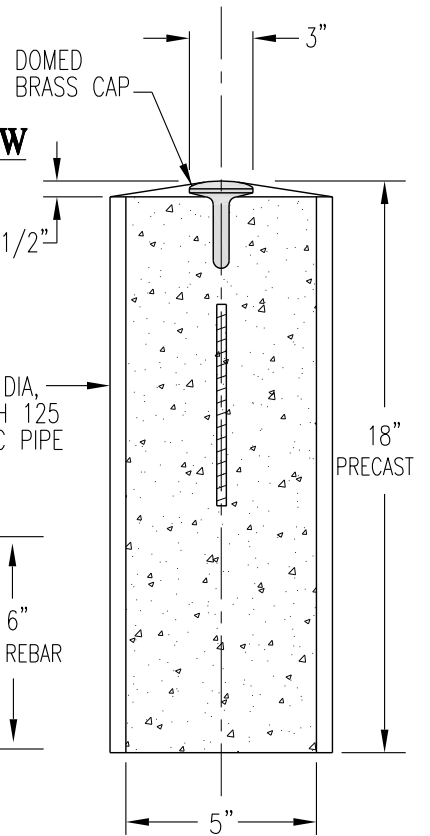
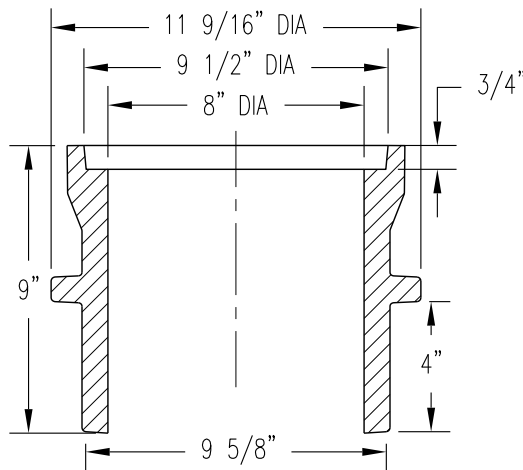


**COVER BOTTOM VIEW**

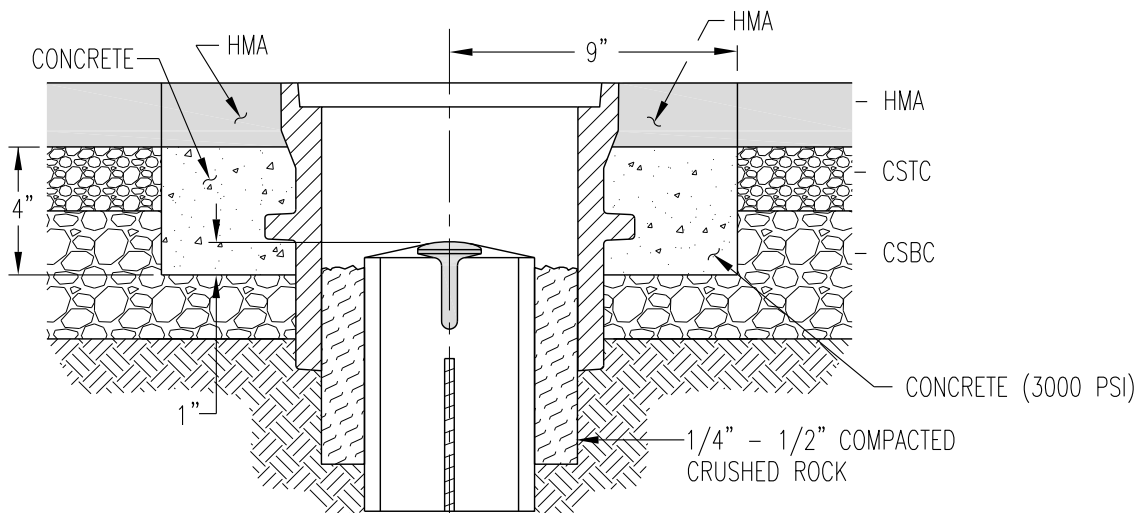
JULIAN DATE TAG & LETTER



**SECTION-FRAME**



**CONCRETE MONUMENT**



**SURVEY  
 MONUMENT**

PUBLIC WORKS ENGINEERING

APPR. BY: PKR

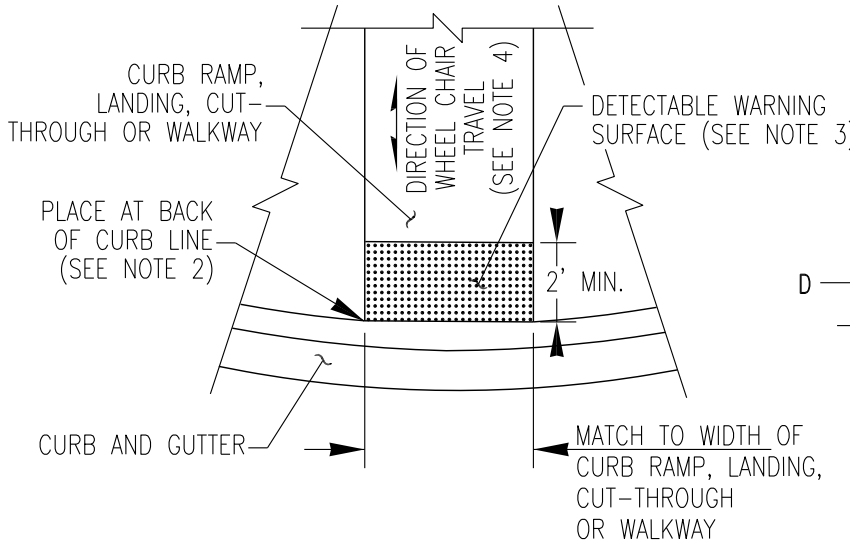
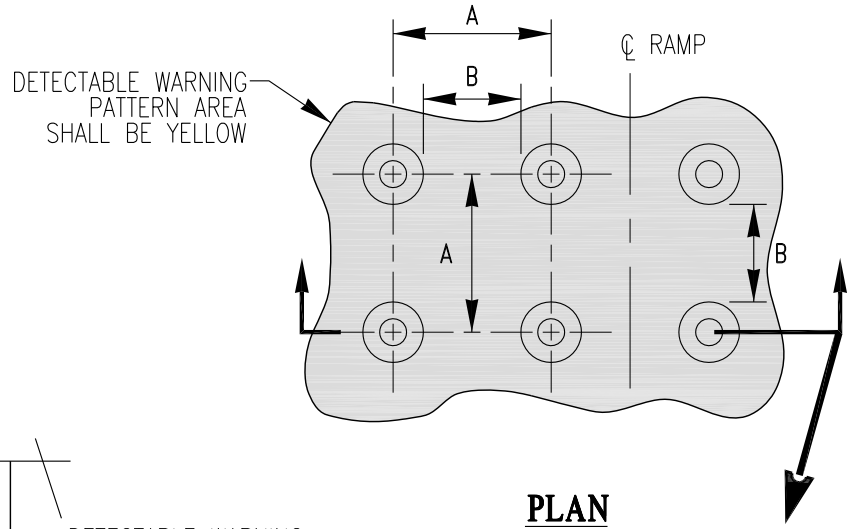
DATE: 07.17

DRAWN BY: LD

DWG: ST20

CAD FILE: 2013\_ST20\_07\_2017

	MIN.	MAX.
A	1.60"	2.40"
B	0.65"	—
C	0.45"	0.90"
D	0.9"	1.40"
E	0.2"	0.2"



**NOTES:**

1. THE DETECTABLE WARNING SURFACE SHALL EXTEND THE FULL WIDTH OF THE CURB RAMP (EXCLUSIVE OF FLARES) OR THE LANDING.
2. THE EDGE OF THE DETECTABLE WARNING SURFACE SHALL BE PLACED AT THE BACK OF THE CURB. CURB SHALL BE FORMED TO PROVIDE A STRAIGHT LINE ALONG THE BACK OF CURB ADJACENT TO THE WARNING PATTERN.
3. THE ROWS OF TRUNCATED DOMES SHALL BE ALIGNED TO BE PERPENDICULAR TO THE GRADE BREAK AT THE BACK OF CURB.
4. THE ROWS OF TRUNCATED DOMES SHALL BE ALIGNED TO BE PARALLEL TO THE DIRECTION OF TRAVEL.
5. SEE STANDARD PLANS FOR SIDEWALK AND CURB RAMP DETAILS.
6. IF CURB AND GUTTER ARE NOT PRESENT, SUCH AS A SHARED-USE PATH CONNECTION, THE DETECTABLE WARNING SURFACE SHALL BE PLACED AT THE PAVEMENT EDGE.

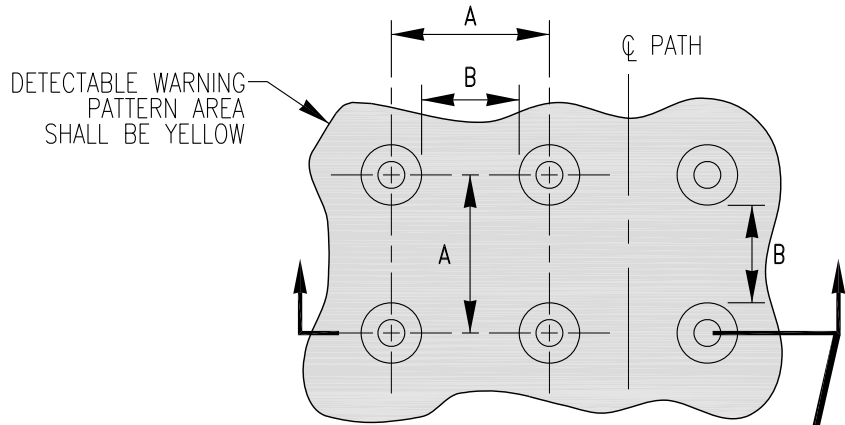


# DETECTABLE WARNING SURFACE

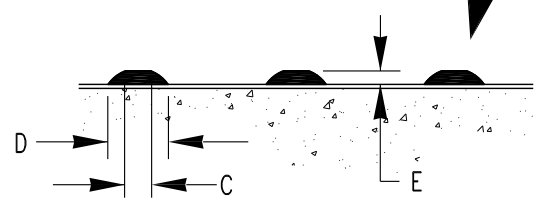
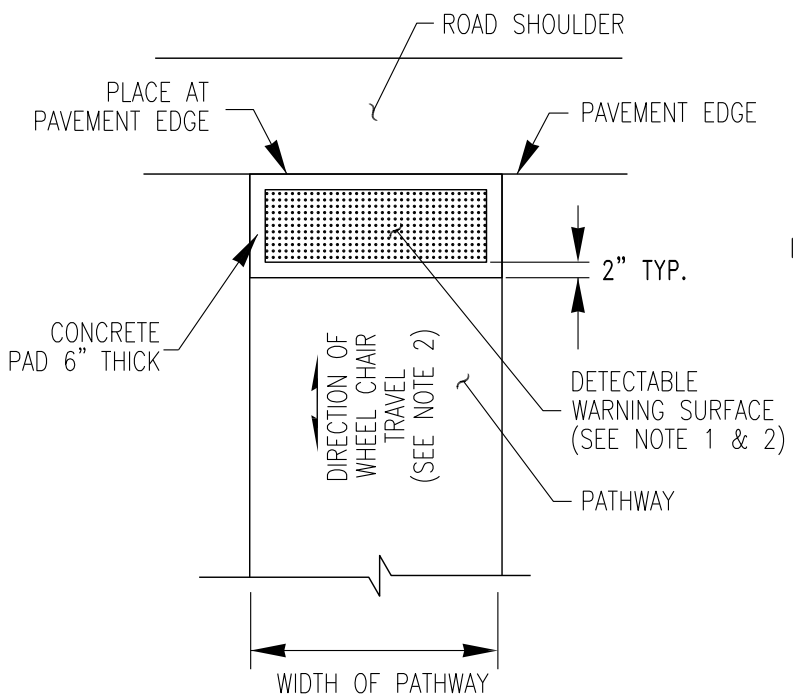
PUBLIC WORKS ENGINEERING	
APPR. BY: PKR	DATE: 01.16
DRAWN BY: LD	DWG: ST21
CAD FILE: 2013_ST21_01_2016	



	MIN.	MAX.
A	1.60"	2.40"
B	0.65"	—
C	0.45"	0.90"
D	0.9"	1.40"
E	0.2"	0.2"



**PLAN**



**SECTION VIEW**

**NOTES:**

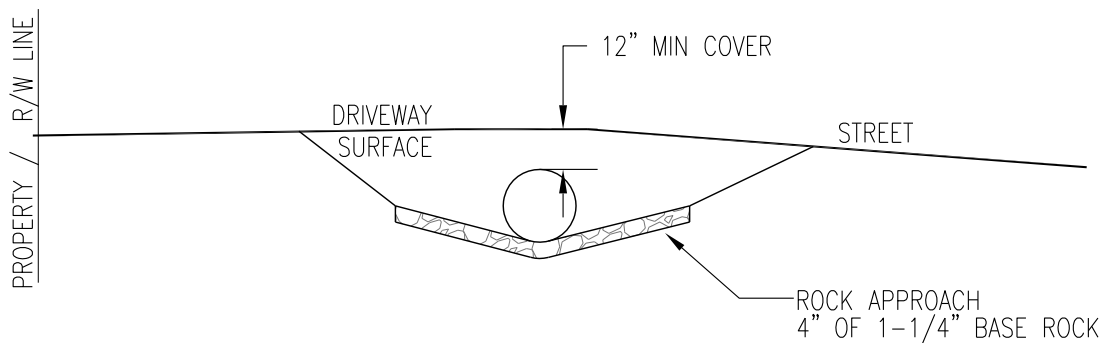
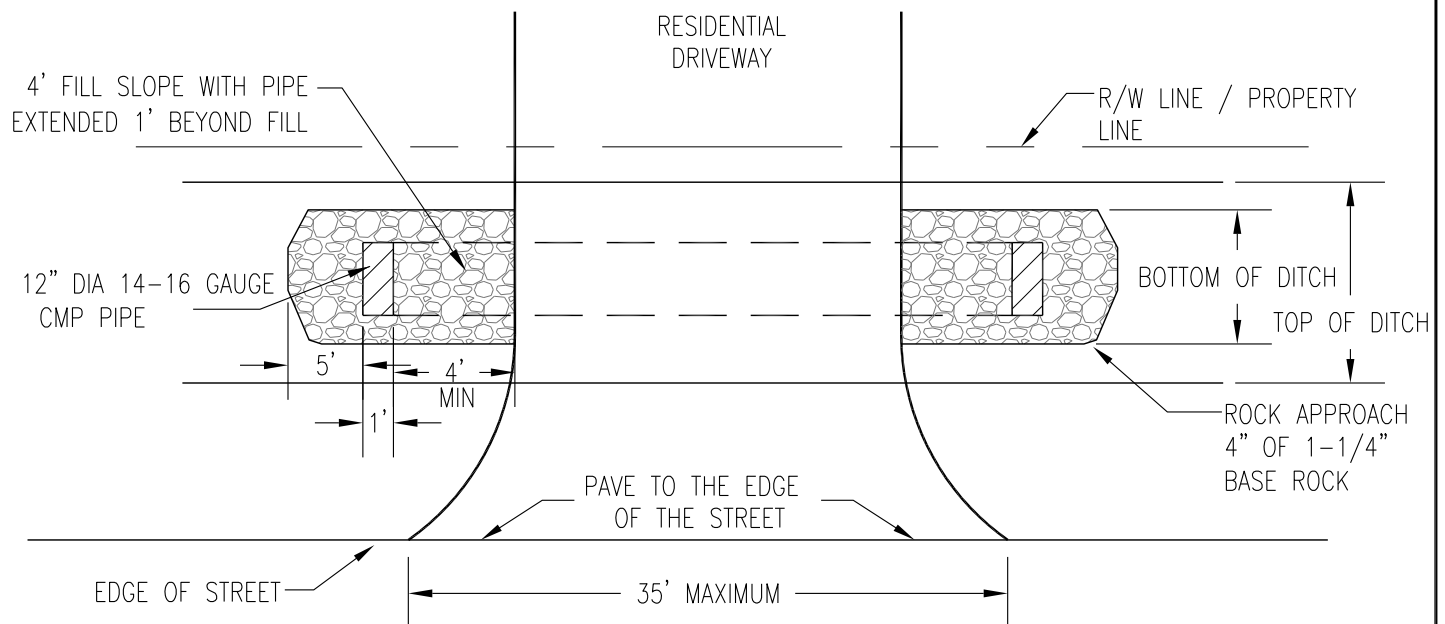
1. THE ROWS OF TRUNCATED DOMES SHALL BE ALIGNED TO BE PERPENDICULAR TO THE GRADE BREAK AT THE EDGE OF THE ROAD.
2. THE ROWS OF TRUNCATED DOMES SHALL BE ALIGNED TO BE PARALLEL TO THE DIRECTION OF TRAVEL.



**DETECTABLE WARNING SURFACE ON ASPHALT PATHWAYS**

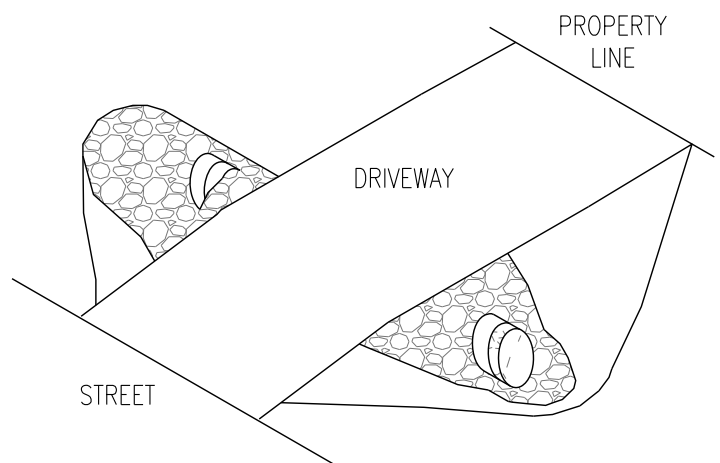
PUBLIC WORKS ENGINEERING	
APPR. BY: PKR	DATE: 09.20
DRAWN BY: EY	DWG: ST21A
CAD FILE: 2013_ST21a_09_2020	

# CULVERT INSTALLATION ALONG CITY OF RICHLAND STREETS WHERE NO CURB AND GUTTER EXIST



**NOTES:**

1. CULVERT PIPE SHALL BE 12" EXTRA STRENGTH CMP.
2. SIDE SLOPES OF DITCH SHALL BE 3:1 MAXIMUM.
3. CULVERT INSTALLATION SHALL PASS A CITY PUBLIC WORKS INSPECTION PRIOR TO FINAL ACCEPTANCE.
4. DITCH SHALL BE RE-ESTABLISHED ACROSS THE ENTIRE FRONTAGE PRIOR TO FINAL ACCEPTANCE.



## RURAL DRIVEWAY / CULVERT DETAIL

PUBLIC WORKS ENGINEERING	
APPR. BY: JR	DATE: 02.12
DRAWN BY: JG	DWG: ST22
CAD FILE: 2012_ST22_02_2012	